



MERREDIN TOWN CENTRE CONCEPT PLAN REVIEW
A COMMUNITY BASED DESIGN INITIATIVE FOR THE SHIRE OF MERREDIN

Prepared for Shire of Merredin, September 2017 with updates December 2020 by UDLA



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Table 2. Document Register

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1.0 INTRODUCTION



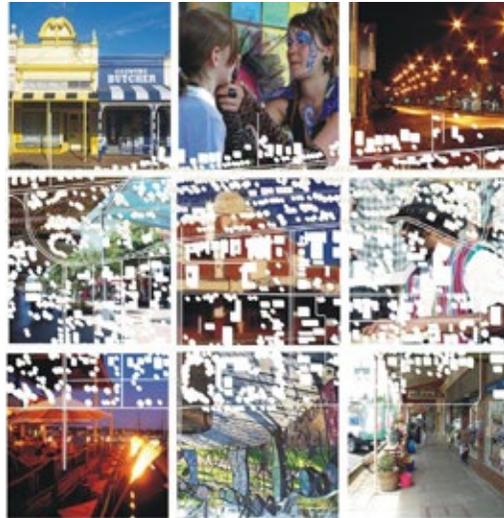
The purpose of the project was to provide an inspired community endorsed Town business centre concept plan. To achieve this aim the Shire of Merredin engaged UDLA* to facilitate a collaborative design process.

OBJECTIVE(S)

The overall objective was to prepare a Merredin Business Centre Concept Plan that was encouraged, directed and developed through a dedicated Business Centre Steering Committee (BCSC) including broader community input, from a dedicated Community Reference Workshop Group (CRWG) and ongoing 'one on one' community champion meetings.

The secondary objectives included a concept plan that:

- Included community backed input and direction;
- Considered local uses, values and embraced a Merredin 'sense of place';
- Considered Visitor attraction and experience;



merredin town centre: have your say

- Considered with regard to practical economic implementation and ongoing maintenance;
- Considered local environmental attributes;
- Is of a high graphical standard, clear

The Shire of Merredin invites members of the community to participate in developing the Merredin Town Centre and Business Precinct.

The aim of the conceptual planning process is to provide firm direction for this important business, community and tourist hub. The conceptual planning will not only focus on the physical outcomes, however is concerned with the future commercial and community vision.

To provide community input and direction into the Town Centre Design, there will be an invitation for community group representatives to participate in design workshop(s) as well as opportunity for general public comment at each stage of the process. The dates are as follows:

(COMMUNITY MEETING) MON 9TH MARCH – SHIRE ADMINISTRATION CENTRE

(COMMUNITY COMMENT) 1ST- 9TH APRIL – SHIRE ADMINISTRATION CENTRE

(REPRESENTATIVE MEETING) TUES 14TH APRIL – SHIRE ADMINISTRATION CENTRE

(COMMUNITY COMMENT) 1ST-8TH MAY – SHIRE ADMINISTRATION CENTRE

(REPRESENTATIVE MEETING) MON 11TH MAY – SHIRE ADMINISTRATION CENTRE

(COMMUNITY MEETING) MON 8TH JUNE – SHIRE ADMINISTRATION CENTRE

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On (08) 90411611 or email admin@merredin.wa.gov.au



- to for public presentation; and,
- Suitable to provide clear direction for community understanding and Shire of Merredin to progress planning approvals

This report graphically documents the

Shire of Merredin, the community and UDLA's four month journey to arrive at an agreed Merredin Business Centre concept direction.

*UDLA collaborated with JBA who provided a focused community planting presentation

1.1 COLLABORATIVE DESIGN PROCESS

AIM

The aim of the collaborative design process was to include the Shire of Merredin, Commercial Stakeholders and interested community members in the concept plan visioning of the Merredin Central Business District and associated open space.

A Business Centre Steering Committee (BCSC) and Community Design Workshop Group (CDWG) were formed to guide and provide participatory input directly into the 3 part design workshop process.

DESIGN WORKSHOP PROCESS

The design workshop process consisted of three parts including:

1. Opportunities and Directions;
2. Exploring Concept Options; and,
3. Draft Concept Plan.

The design process did not come empty handed and drew on previous studies and reports on the townscape, talking / yarning with local people and conducting community meetings with consistent feedback opportunities to develop the idea of Merredin's town centre potential.

The process allowed for the organisation of community representative group to act on behalf of different sectors of the community and the general public had opportunities to provide direct feedback at different times throughout the three part design process.

Each meeting and design workshop was documented through note taking, photographs and mapping. The information was used to produce plans and concepts, all which were presented back to the community representative group at the following meeting.

COMMUNITY ENGAGEMENT

The Shire of Merredin suggested a 'low key' public meeting for interested public members as the most appropriate way to commence and inform the wider community on the Town Centre Concept Design opportunity.

This public meeting was able to provide the aims and objectives and information around the upcoming collaborative design process in turn garner ongoing interest for community members and provide the opportunity for authentic participation.

The community engagement process included the following three phases:

Phase 1: A community briefing outlining project and proposed process and confirming the existing conditions

Phase 2: From the community briefing a CDWG was formed that mirrored the BCSC three workshop design process. The BCSC was in itself consisted of members willing to represent a local Merredin stakeholder, community or interest group.

Phase 3: Following Concept Design development (i.e. 3 x design workshops) a final public briefing invited community feedback on the Draft Concept Plan was presented by the project team and prepared through the inclusion of the BCSC and CDWG.

The final concept plan essentially provided direction for:

- Town beautification;
- Visitor interpretation and wayfinding;
- Pedestrian and car movements / parking; and,
- Spatial and event programming.





2.0 PHASE ONE: CONTEXT



2.1 MERREDIN IN NUMBERS

The following quantitative desktop studies purpose was to determine Merredin's immediate physical, social, economic and environmental factors that influence the town's present context and future design of the CBD.

LOCATION

- Approximately 260km east of Perth
- Shire = 3,372km²
- Located on Great Eastern Highway
- Railway Facilities (Prospector and Avon link)

WEATHER

- Mediterranean climate
- Hot, dry summers
- Mild winters
- Average maximum temperature - 24.7°C
- Average minimum temperature - 10.6°C
- Dec - Feb low 30's or above
- Wettest months usually May -

August

INDUSTRY

- Major commercial / retail centre
- Regional base for a range of government agencies and services
- Region known for agriculture
- Approx. 40% of WA's wheat production from Merredin (100km radius)
- Major industries include (ID Community 2011):
 - Agriculture (13.1%)
 - Retail (11.5%)
 - Education and Training (9.5%)
 - Transport or storage (8.7%)

DEMOGRAPHICS

- Population = 3,350 (2016) increase from 3,281 (2011)
- Current population = 3,350
- Aboriginal population 234 (2016) 7% of Merredin Shire population
- Number of dwellings 1,648 (2016)

- Age structure differs from Regional WA - a higher % of people in Merredin aged 60 years and over (2016)

- Extensive community involvement

LANDSCAPE

- Undulating slopes
- Granite tors
- Mosaic of plant habitats
- 960 recorded species of plants
- Extensive wildflower variety
- Sand plain and clay soils
- Key environmental issues - loss of biodiversity / salinity
- Water security issues



2.2 EXISTING CONDITIONS

RECREATIONAL AREAS

Generally considered to be plenty of open space. However, spaces are not linked and many are not set up to clearly identify themselves as useable active or passive recreational areas. General consensus is they need upgrading and consolidating but are a very important part of Merredin.

WATER HARVESTING

Merredin is on the leading edge in sustainable water saving practices. These are integral innovations for the Town's survival and also offer valuable opportunities for interpretation.

TOWN CENTRE

Is the arrival point for local communities and passing tourist traffic. Barrack Street was planned as a major service road. Traditionally, Bates Street would have been considered the central main street, with the centre hub being the Mitchell and Bates Streets intersection. Barrack Street has developed as the main street with the new hub located at the intersection of Bates and Barrack Streets.

CULTURAL

Merredin has a strong agricultural

history as well as connections to WWII. The town is lucky enough to still have local indigenous representation and an opportunity exists to tell these stories through interpretation and linking the important historical and cultural places.

TOURISM

Merredin's location makes it a popular stop off point and has been highlighted as a hub for surrounding towns in the wheatbelt. In its own right Merredin has attractions for a tourist destination but currently does not capitalise on this market. By recognising the benefits of tourism, the town will ensure longevity and improve facilities for local residents.

TOWN ENTRIES

Due to re-routing of the main highway there is a general feeling that the town entries are not sufficient. There have been Investigations into alternative tourist route entries that access the old highway from the outskirts. Outstanding town entries only work with equally great town centres. Existing town entries may benefit from works such as lighting, planting and cleaning up (e.g. the removal of run down houses opposite the entry).

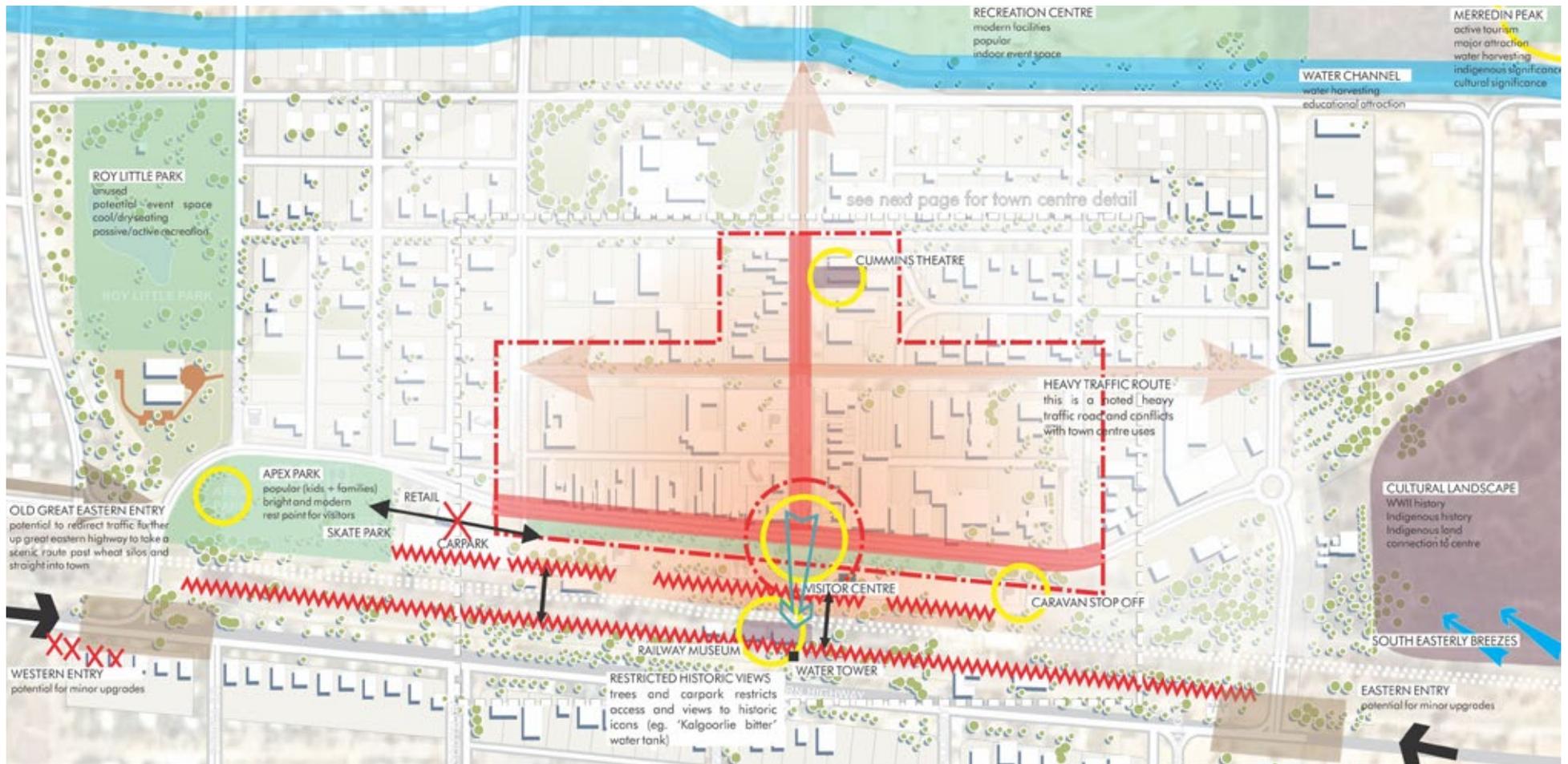
LAND USE

Separating light industrial out of the town centre avoids conflict between large vehicles and pedestrians. The town centre should aim to provide light commercial, retail and hospitality services. Tourism accommodation to be encouraged close to the centre. Any permanent residential uses should not be discouraged as it adds to activity and security at night but should be above or behind an active business facade.

DEMOGRAPHICS

A mix of people. The population swells during harvest and more people are coming into town due to construction and educational opportunities. There is a general feeling that community spirit has eroded in the past 15 years therefore an opportunity exists to encourage community meeting spaces and projects. There is a changing workforce climate where some community members remain unemployed for extended periods. However, all will continue to contribute to the towns economy. It has been acknowledged that the 65+ age group are looking to Merredin for a lifestyle change yet require facilities, amenity and security.





STREETSCAPE

Merredin's Town Centre (CBD) is fortunate to have 'good bones', with a main street, Barrack Street conducive to community interaction including activated strip shopping. In contrast blank walls, unused spaces or vacant shops are an immediate detriment to town centre vibrancy. Consider public art, temporary 'pop up' stores or lower rent incubator business opportunities to connect these unused spaces and enhance the main street vibrancy and experience.

OPEN SPACE

Plenty of open space that requires greater formalisation and linkages. The lawn area on the southern verge of Barrack Street is viewed as a softener to the streetscape and could be better utilised through use of spatial program and pedestrian amenity.

ROADS AND PARKING

Merredin's streetscape character consists of wide roads with ample parking. There are limited traffic slowing devices such

as change in pavement, pedestrian crossing or speed inhibitors. Median strips incorporated within Barrack and Bates Streets help provide traffic friction and a pedestrian friendly environment. Additional road pavement treatments could be used to indicate the CBD precinct. Street parking, in particular angled parking reduce main street speeds through the action of vehicles pulling in and out of traffic. There is sufficient parking within the town centre to meet current and foreseeable demands.

NIGHT SCAPE

Merredin currently does not cater strongly for a nightlife experience such as the lighting of historic architecture, public art and feature trees.

PATHS

Barrack Street has wide walkways with verandahs providing protection from the elements for pedestrians. Chance places for people to rest and meet are limited and these currently occur mainly on the grassed areas on Barrack Streets

southern verge. There are very limited alfresco dining opportunities to enliven the street.

TOURISM

Community and tourist needs are similar when providing comfortable meeting places. Visitors unfamiliar with the area are often confused by entry roads into the CBD and 'point of arrival'. The provision of pedestrian amenity and meeting places allow for friendly interaction with locals; a unique experience associated with Merredin.

ARTWORK AND COMMUNITY

Public artwork such as high quality mosaics are located around the town but very little community, cultural or interpretative art is on display in the town centre. Public art must be easily accessible and in prominent positions.

COMMUNITY NEWS / TOURISM

Merredin is a friendly place and interaction often occurs at the Barrack and Bates Street intersection, including the southern grassed verge, making it an

essential activity node.

CULTURAL HERITAGE

Strong support to integrate cultural history in regard to farming, WWII and in particular indigenous stories and connections to country.

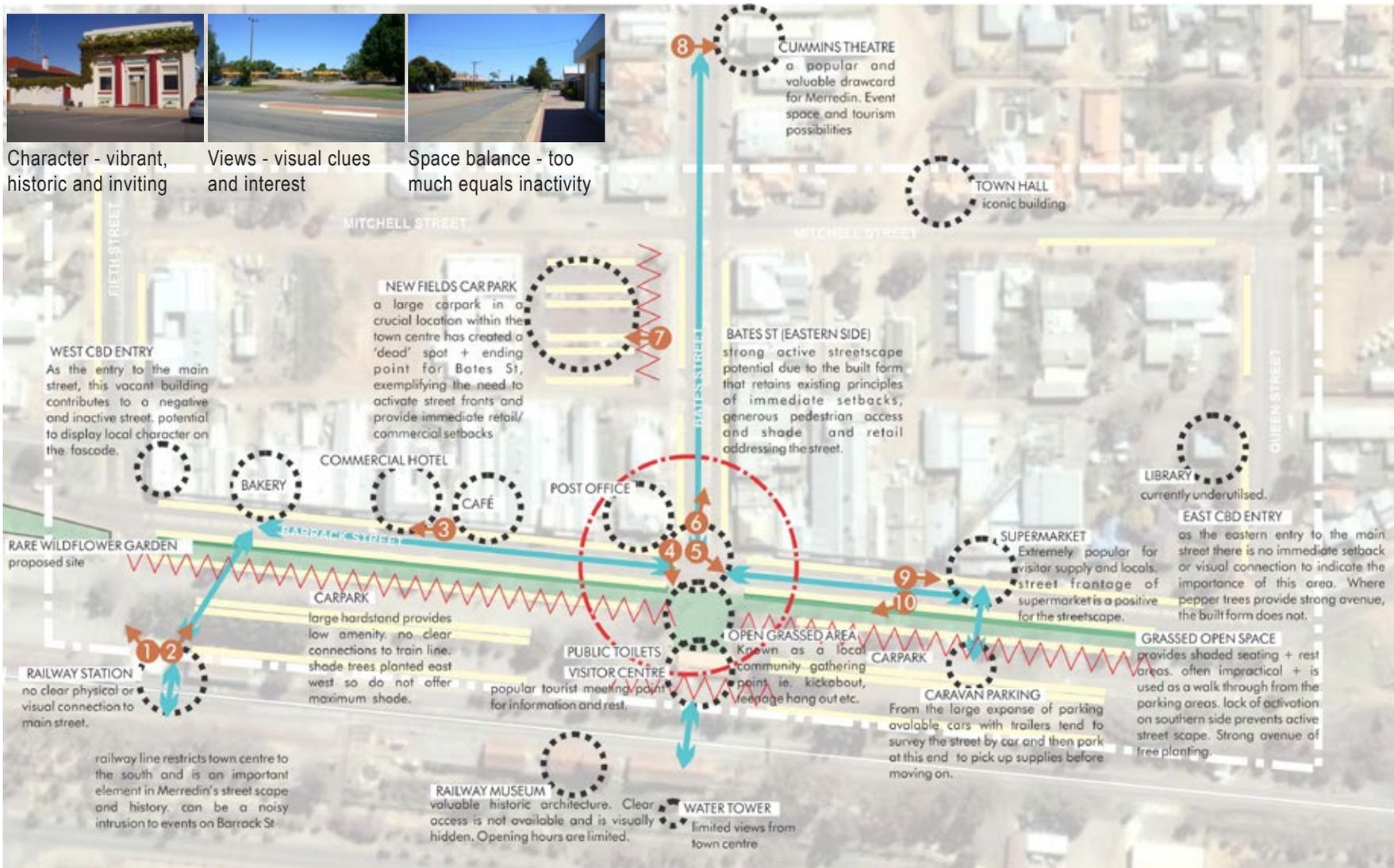
EVENT SPACE

A mild climate allows for outdoor activities for most of the year however there is limited outdoor event space. The intersection of Barrack and Bates Streets has become the node for community events (e.g. late night shopping, boot scooting and Carols by Candlelight). The area is not set up formally to host these events.

MEETING PLACES

The café and bakery is popular for residents to meet and chat, the supermarket for supplies (tourists and locals). The visitor centre and post office are regular 'meet and greet' locations. Kids like to hang out in the shade of the trees on the outskirts of the grassed area.





2.3 MERREDIN TOWN CENTRE

Community and stakeholder members were asked questions to gain contemporary information regarding the towns existing context.

Each participant was provided with a plan (Existing Conditions Plan - shown on adjacent page) to provide graphical notes to document their understandings of how the town centre is presently used.

MERREDIN IN YOUR OPINION

- What is the perceived centre now and why?
- Favourite place to hang out and why?
- What is missing most?
- Which roads work the least and why?
- How do tourists know where to turn?
- Where do tourists turn and drive directly to?
- Where do tourists visit after the initial stop?
- Biggest eyesore

- Favourite view
- Where do you spend most of your time in town?
- Perceived main strip / centre?
- Event space?
- Usage of space between the railway + Barrack Street
- Areas of density and usage?
- Vehicle and pedestrian routes?





Example of Community - Existing Conditions Exploration

2.4 PREVIOUS STUDIES

Previous townscaping plans and educational research were explored to provide learned input into the project.

TOWNSCAPE PLAN - MICHAEL TOOBY AND ASSOCIATES

MAY 2002

IDENTITY

Red tail cockatoo preferred / First shire in WA / Largest inland CBH in southern hemisphere / Railway or pipeline feature / merit tree / "Heart of the Wheatbelt"

COLOUR SELECTION

Green with Cream stripe / Blue + Gold/ Heritage colours

APPROACHES AND ENTRIES INTO TOWN

West side approach needs vegetation / Screen railway yards with trees / large welcome sign needed / Duplicate west and east entries to town / information bay needs revamp / Information bay to heritage precinct (possibility) / Move fountain to Roy Little Park

BARRACK STREET

Shade trees / Parking facing east west / Footpaths extended for outdoor dining / No angle parking

ARRIVAL POINTS

Make Pioneer Park a main point

BATES AND MITCHELL STREETS INTERSECTION

Build up intersection

VEHICLE CIRCULATION

Need alternative truck route / Designated bus route

ROSE PLANTING

Reduce number of rose gardens / Consolidate in main areas, focal points / more low maintenance gardens

PEDESTRIAN SPACES AND ROUTES

More and better seating in shade / trees on islands / Safer pedestrian crossings / Need better signage

ROY LITTLE PARK

More trees not to obstruct security lighting / More BBQS / Ornamental gardens / Chairs and bins / Gazebo

SIGNAGE

Update welcome signs / Pictures of parks at info bay

HERITAGE BUILDINGS AND BUILDING STOCK

Cummins Theatre facade - beer garden

PROPOSED COMMERCIAL SUBDIVISION

Council currently progressing with development

OTHER / COMMENTS

Local business chance to quote new street furniture / Bates Street needs to be included in town planning / Disabled access to be considered / Consult with public for ideas and details

TREES

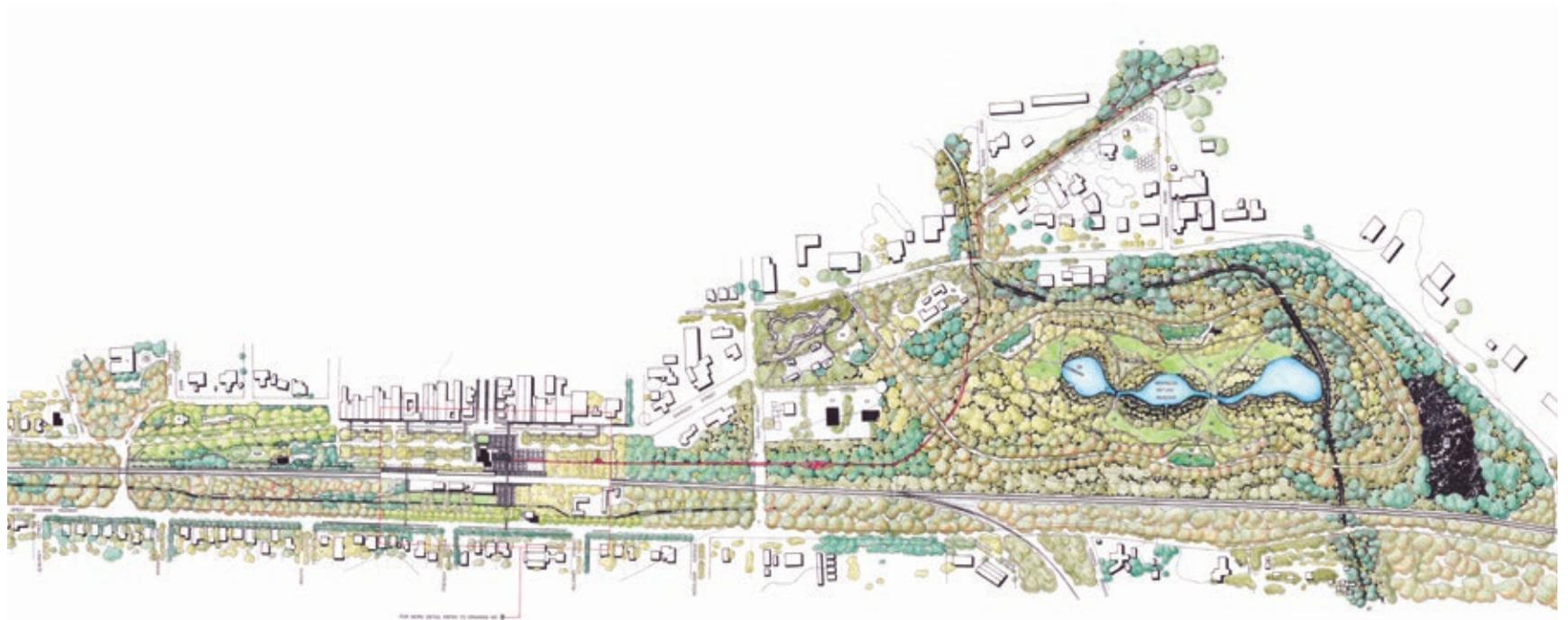
Need large trees / Drought and salt tolerant / Flowering species / No lilac or pepper trees / Salmon gums and other natives to WA



MERREDIN TOWN CENTRE - SARAH HAWKE, UWA HONOURS

2006

- LEGEND**
- New Building
 - Lake
 - Green
 - Scenic and Big Planting
 - Proposed Existing Woodland Planting
 - Mark Way History Trail linking the town centre and Merredin Park
 - Pedestrian Pathways: crushed granite, gravel
 - E. eucalyptus - Gully Gum
 - E. acuminata - Tree Gum
 - E. rhodantha - Sugar Gum
 - E. seneculoides - Great Red Gum
1. Council Offices
 2. Relinquish of Barack Street proposal and entrance to town
 3. State Park
 4. State Park Car park
 5. Apex Children's Playground and Parkland
 6. Proposed existing woodland planting
 7. Seaton Gully along Great Eastern Highway
 8. Mark Way History Trail linking Town Centre and Merredin Park
 9. Drop-off/pick-up area for Walk Trail, accommodate visitors and pedestrians, shade, seating
 10. Rail Station with Book Pavilion and Seating
 11. Proposed Light Industrial
 12. Merredin Emergency Fire Service
 13. BWS Childrens' shelter, abattoir block
 14. BWS Truck
 15. Recreation and Public Space
 16. Carpark
 17. Scenic Drive: drive through hills, Merredin Reserve
 18. Merredin Military Reserve Carpark
 19. Merredin Military Reserve
 20. Carpark
 21. Pedestrian Pathways: accessible to wheelchair
 22. Greened Recreation / Play / Public Space
 23. Seaton Street Bush walk
 24. Goodsie Shopping Centre
 25. Abattoir Block
 26. Drainage Channel
 27. Greenline
 28. Rail Area
 29. Tree Rainwater: planting windbreaks, green and water landscape initiatives throughout the town
 30. Water Stables: offering commercial and retail along Merredin Way through the Merredin Reserve and around Merredin Park



3.0 PHASE TWO: OPPORTUNITIES AND DIRECTIONS



The Opportunities and Direction Phase explored aspirational, yet achievable directions for Merredin. Following our stakeholder and community meeting these prospects were listed within the following themes:

- Town Centre, Streetscape, Open Space, Event Space and Meeting Places, Public Art and Community Projects, and Tourism.

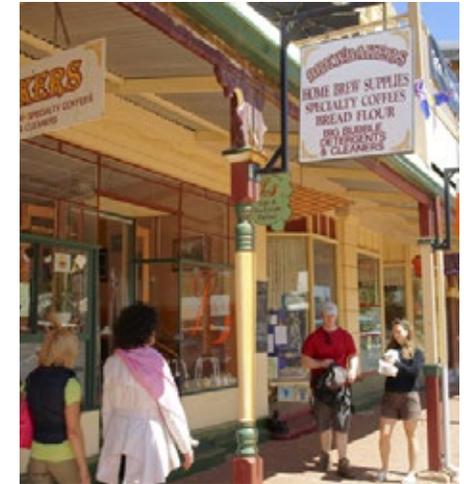
3.1 TOWN CENTRE

- Recognise as a regional centre
- Capitalise on its position as a major hub and stopover point
- Clarity to the Centre of town to maintain ease of use
- T junction of Barrack and Bates Streets as point of arrival and event space
- Barrack Street is main street
- Secure spaces with views out
- Encourage walkability
- Provide spaces for program (e.g. spaces for music to be played at markets)
- Artwork to excite, educate and enliven



3.2 STREETScape

- Fill in gaps - condense retail
- Alfresco dining
- Remove / renovate derelict or 'tired' buildings
- Create sheltered spaces from railway to reduce noise
- Open and reveal portions of the railway line for interest
- Strip shopping
- Designated caravan / tourist parking
- Cater for aged and gopher parking
- Consolidate for clear parking zones
- Tighten streetscape to make a more pedestrian oriented centre
- Needs traffic calming devices and no trucks in centre
- Widen footpaths to allow alfresco dining or interaction
- Spaces to sit / watch / meet



3.3 OPEN SPACE, EVENT SPACE, MEETING PLACES

- Clear connections
- Consolidate and program spaces
- Comfortable and shaded seating / meeting places in applicable areas
- Clear walkways to specific locations
- Potential market space, event space
- Maintain high quality open space and trees to soften streetscape
- Upgrade library for more current and alternative uses
- Relocate library to a more central location
- Capitalise on mild weather and opportunity for outdoor activity



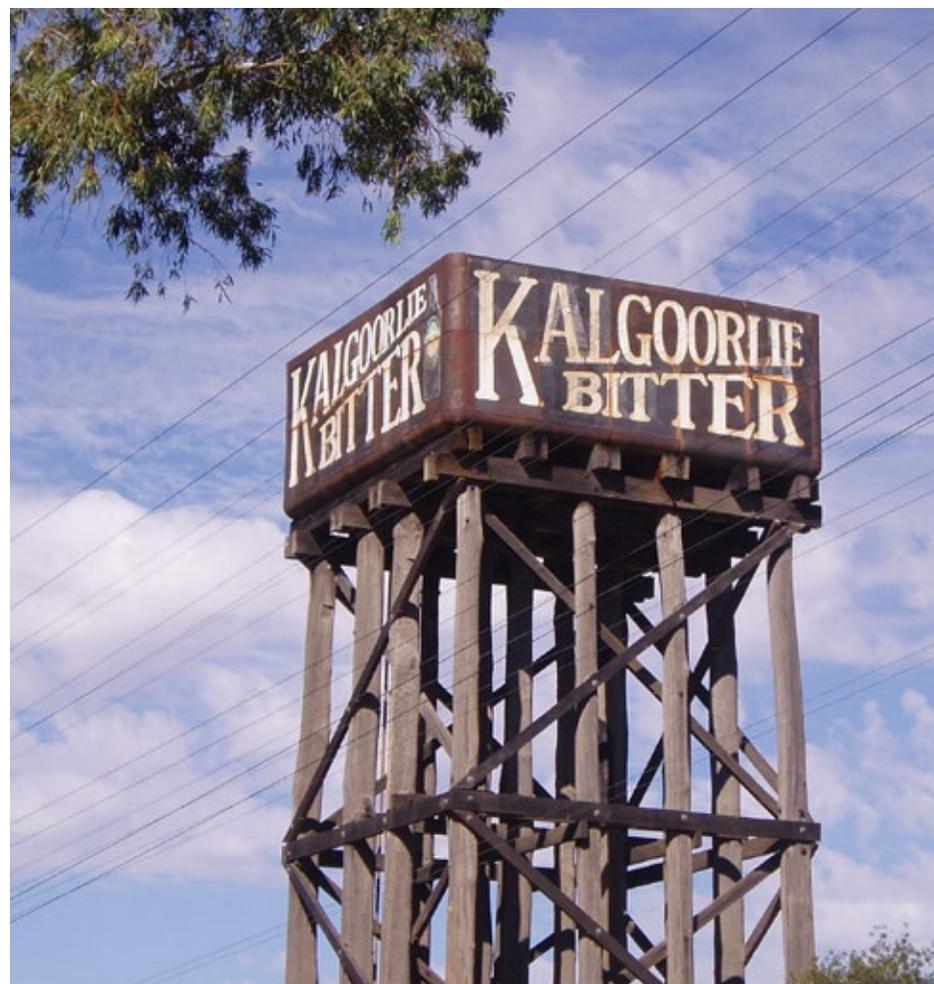
3.4 PUBLIC ART AND COMMUNITY PROJECTS

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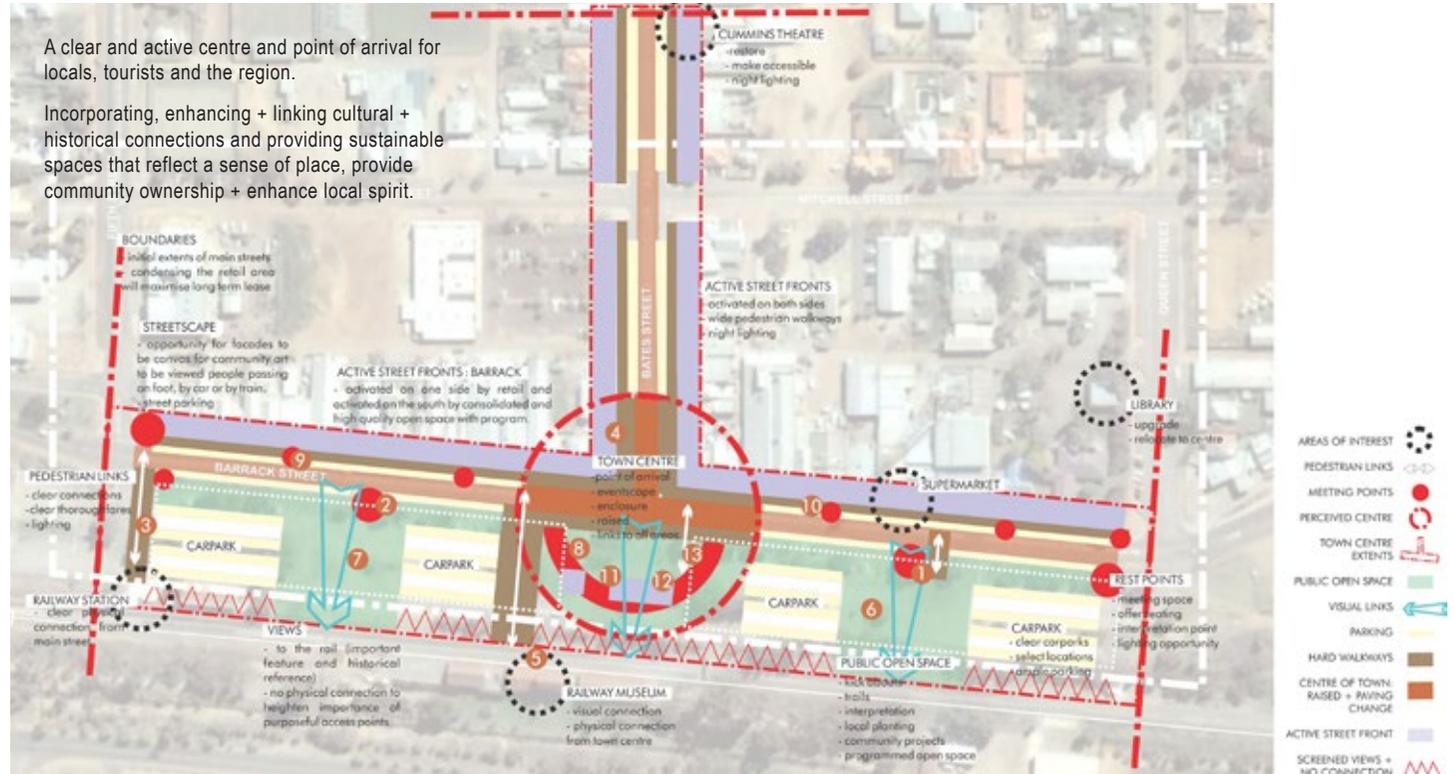


3.5 TOURISM

- Maintain and build on spaces that encourage interaction
- Cater for tourists and particularly people with caravans
- Link popular stop off points and make the links walkable
- Improve Visitor centre surrounds as a rest area and major stop off point
- Simplify entry and clear visual cues
- Make the spaces age friendly- particularly for the older travellers
- Ensure the Railway museum is open maximum hours by incorporating another business with this (e.g. Visitor Centre)
- Clear connection to Railway museum
- Keep it simple
- Public art
- Public and community events
- Clear direction for people entering Town centre



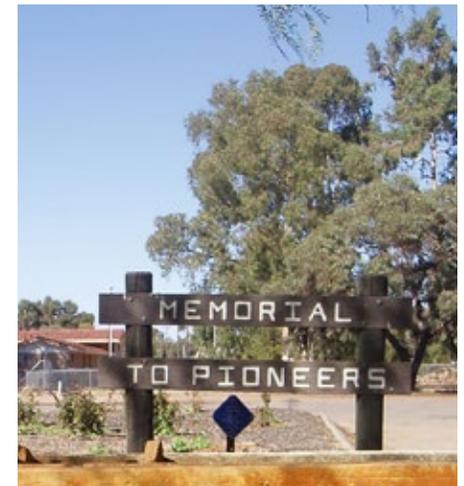
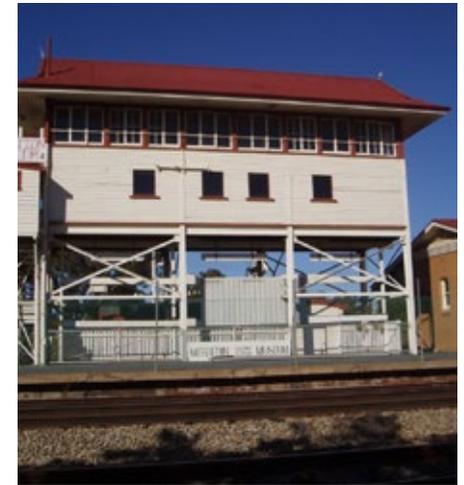
3.6 OPPORTUNITIES AND DIRECTIONS PLAN



3.7 MERREDIN'S POTENTIAL

Similar to the Existing Conditions Design Phase the aspirational question 'How do you imagine Merredin in years to come?' was explored. The questions investigated different types of town centres through precedent studies.

- What is the future of Merredin?
- Experience of the space as a visitor or a resident?
- Business / Heritage / Agricultural / Tourism district?
- Meeting places and Cultural centre?
- Shopping centre v's main street
- Large carpark v's street parking
- Open space / event space
- Transport route
- Demographic
- Community artwork
- Regular v's weekly use
- Road and path widths
- Street amenity - trees
- Lighting and night activities.....



RETAIL

Complex shopping v's Street frontage shopping



ROAD

Widths
Median strips
Off street parking
Large carparks
Shade
Roundabouts



PATHS

Widths

Dual use

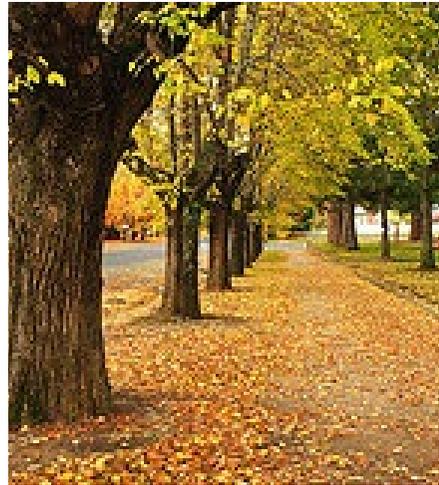
Artwork

Meeting spaces

Shade

Links / access ways / trails

Popular routes



EVENT SPACE

Close off streets

Events space

Festivals / fetes

Concerts

Weddings

Large v's small events



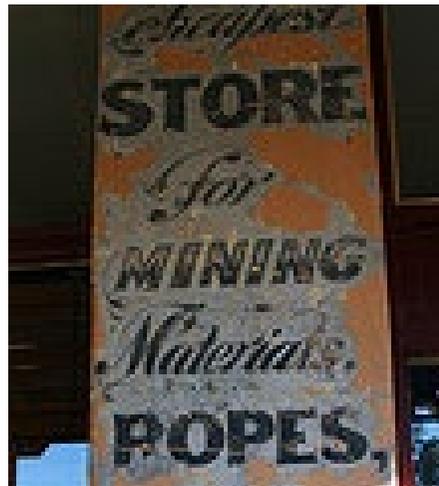
COMMUNITY NEWS

Meetings spaces

Noticeboards

Banners

Signage



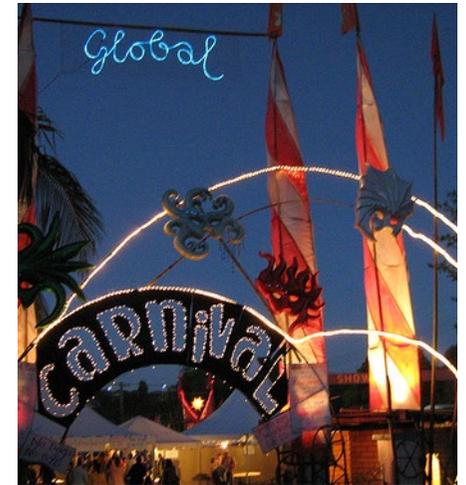
NIGHTSCAPE

Pubs

Dining

Lighting

Events



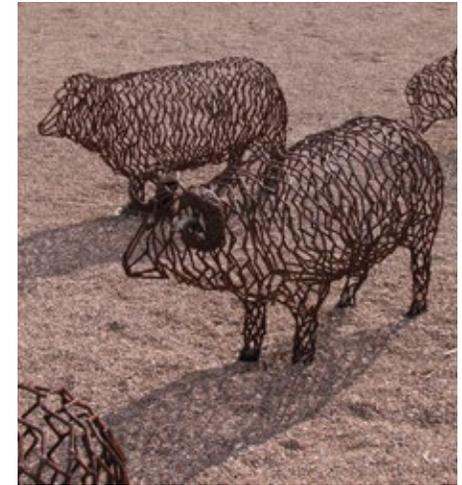
MEETING PLACES

- Street
- Building
- Park
- Local faces



ARTWORK AND COMMUNITY

- Street furniture
- Installations
- Icons
- Art grants



OPEN SPACE

Events

Children / Adults

Active / Passive

Programs

Greenery

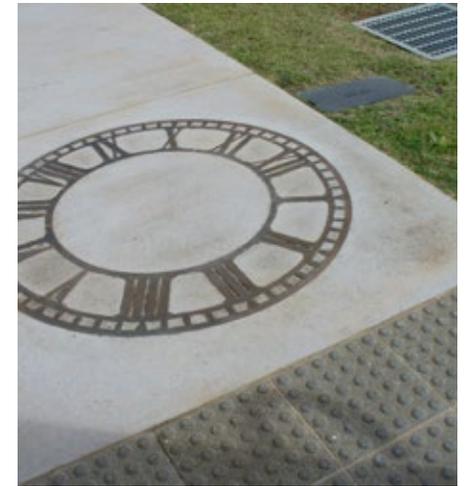


HISTORIC PRECINCT

Historic buildings

Clues in the landscape

Trails



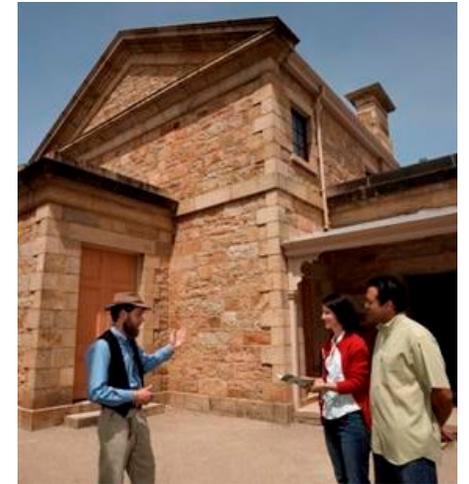
BUSINESS PRECINCT

- Easy access
- Rest areas
- Lunch bars
- Practical space
- Presentation
- Pedestrian and traffic interface
- Shade / protection



TOURIST ATTRACTION

- Tourist centre
- Visitor centre / Information Bay
- Street
- Building
- Park



AGRICULTURAL CENTRE

- Sheds
- Wide roads
- Vehicle priority
- Reference to agriculture
- Framed views of active industry
- Screening inactive areas



DEMOGRAPHICS

- Children
- Families
- Retirees
- Workers
- Artists
- Travellers
- Day trippers
- Motorcycle clubs
- Defence force
- Students



3.8 EXAMPLE TOWNSCAPES - YORK, WA

Stakeholders and community members were questioned on existing townscapes and streetscape initiatives considered favourable and could apply to the Merredin context. The following towns precedents were chosen with regard to historic and friendly strip shopping, parking and spatial programmes. These were explored extensively through graphical images and discussions of local programme and visitor attraction. The following images are a selection from these subject town precedents.



KINGAROY, QLD





4.0 PHASE THREE: CONCEPT OPTIONS



4.1 OPTION ONE

During the design process, the design drivers (themes) revealed themselves as:

- Point of arrival
- Programming open space
- Consolidating and ordering car parking
- Encouraging activation
- Investigating Merredin's sense of place (real order)
- Linking visually, physically and culturally
- Community ownership (pushing the process)

Three options were developed that included options within options to explore and discuss their consideration for Merredin.

The following explores the opportunities and directions of each option for stakeholder, community and project team consideration.

From these options a Merredin Business Centre draft concept plan would be developed.

1 - ENTRY

EXISTING

An immediate upgrade could be the removal of the derelict buildings across the highway.

FUTURE

Future investigations into an entry feature could include a landscape treatment that provides a contrast to the dense local planting behind and displays the region's unique flora or agricultural produce. e.g. A wheat clearing is a simple statement on Merredin as an agricultural centre in the Wheat belt.

TOURIST ROUTE

Potential to guide tourists into town by means of a tourist route turn off further up the road leading vehicles along the old Great Eastern highway entry.

2 - POINT OF ARRIVAL

CENTRAL ISLAND

The centre of town is heightened by a raised central island and the use of contrasting pavement. During major events the intersection could be closed

to vehicles, opening the space up for performance.

STREET ACTIVATION

Building on existing structures, retail and restaurants activate the southern side of Barrack Street around the point of arrival.

PLAZA

Breaking through the buildings is a plaza area that not only opens views and connects the highway to the town centre, providing space for events and interpretation.

PLANTING

Endemic plants play a secondary role as a backdrop to the hard urban spaces, however the thick planting defines and shades the perimeter of the plaza.

WATER TOWER

The water tower becomes a feature in full view from the highway, the town centre and forms a focal point at the end of the pathway from the supermarket.

EXISTING FACILITIES

The public toilets and Visitor centre to be incorporated into the southern street activation.

OLD RAILWAY STATION

Located behind the 'activated' intersection, the railway buildings assume a secondary role as a feature in the parkland and from Great Eastern Highway. Potential to upgrade and retain its primary use as a museum with other uses incorporated such as men's shed. Potential to clean up museum and relocate some robust interpretation to the plaza and other artefacts could be displayed in shop fronts around the to form part of a walking museum tour.

3 - PARKLAND

Opening up large areas provides clear access to south of the railway line and opportunity for various open space for active/passive recreational activities.

4 - TRAIN STATION

ACCESS

Pedestrian area and walkways to Barrack Street provide visitors with clear direction to the town centre. Parking close by allows for easy pick up and drop off.

PLANTING

The large fig trees are retained as a feature at the station. A backdrop of local planting on the south side of the line provides a sense of place at a major point of arrival and departure.

5 - LOCAL PLANTING

Large tracts of local planting (Gimlets and Salmon gums) separate the north south access ways and provide a contrast to the open lawn areas. The endemic flora adds to the local sense of place, may attract local bird life such as the black cockatoo, and also acts as a feature for tourists to the area.

6 - PARKING

WESTERN CAR PARK

Consolidated parking for cars using Apex park, the café or exploring the western end of town.

TRAIN CAR PARK

Parking for people using the train or the western end of town. Coach bays are provided under the fig trees.

EASTERN CAR PARK

Consolidated parking for the east end in particular for the supermarket.

LARGE VEHICLE CAR PARK

Provides short term parking for people driving through the centre of town with trailers and caravans.

STREET PARKING

Retain street parking in the form of parallel and angled.

STAFF AND EXCESS SHOP PARKING

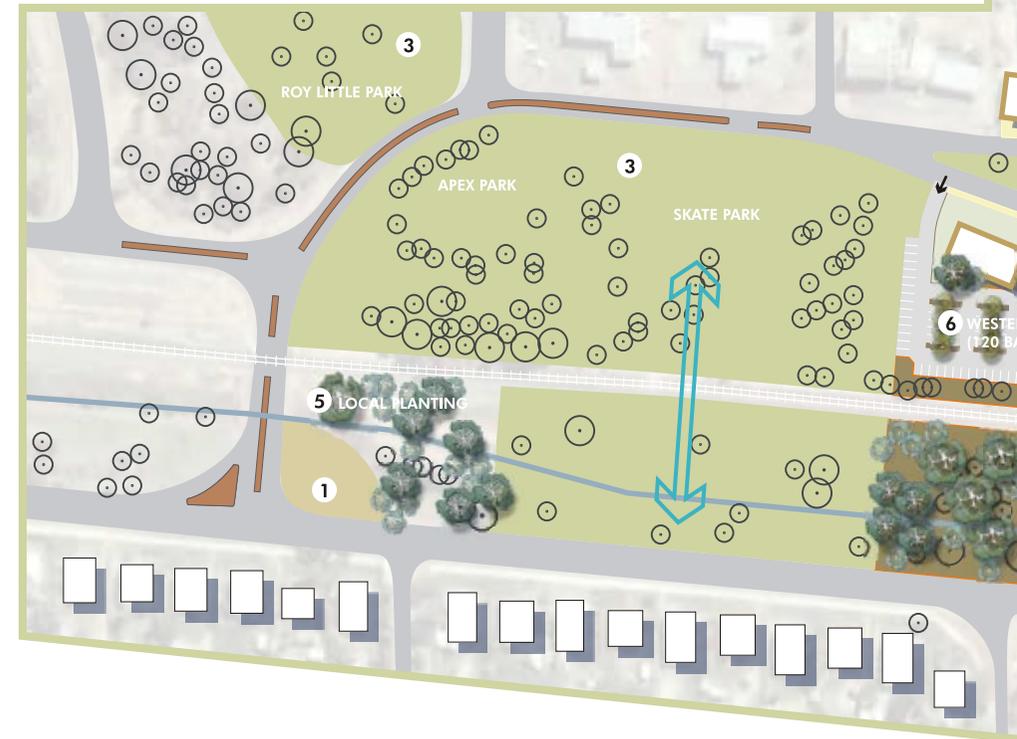
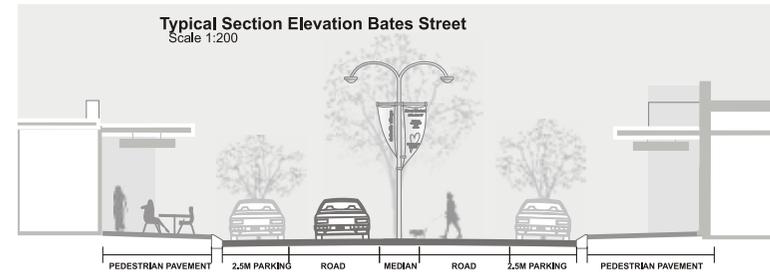
Any excess shop parking or staff parking should be provided at the rear of buildings. Do not locate large car parks on street frontages as this will only stop pedestrian flow along the street.

PULLOVER BAYS

Parking bays at the plaza allows cars to stop, explore and take advantage of any photo opportunities.

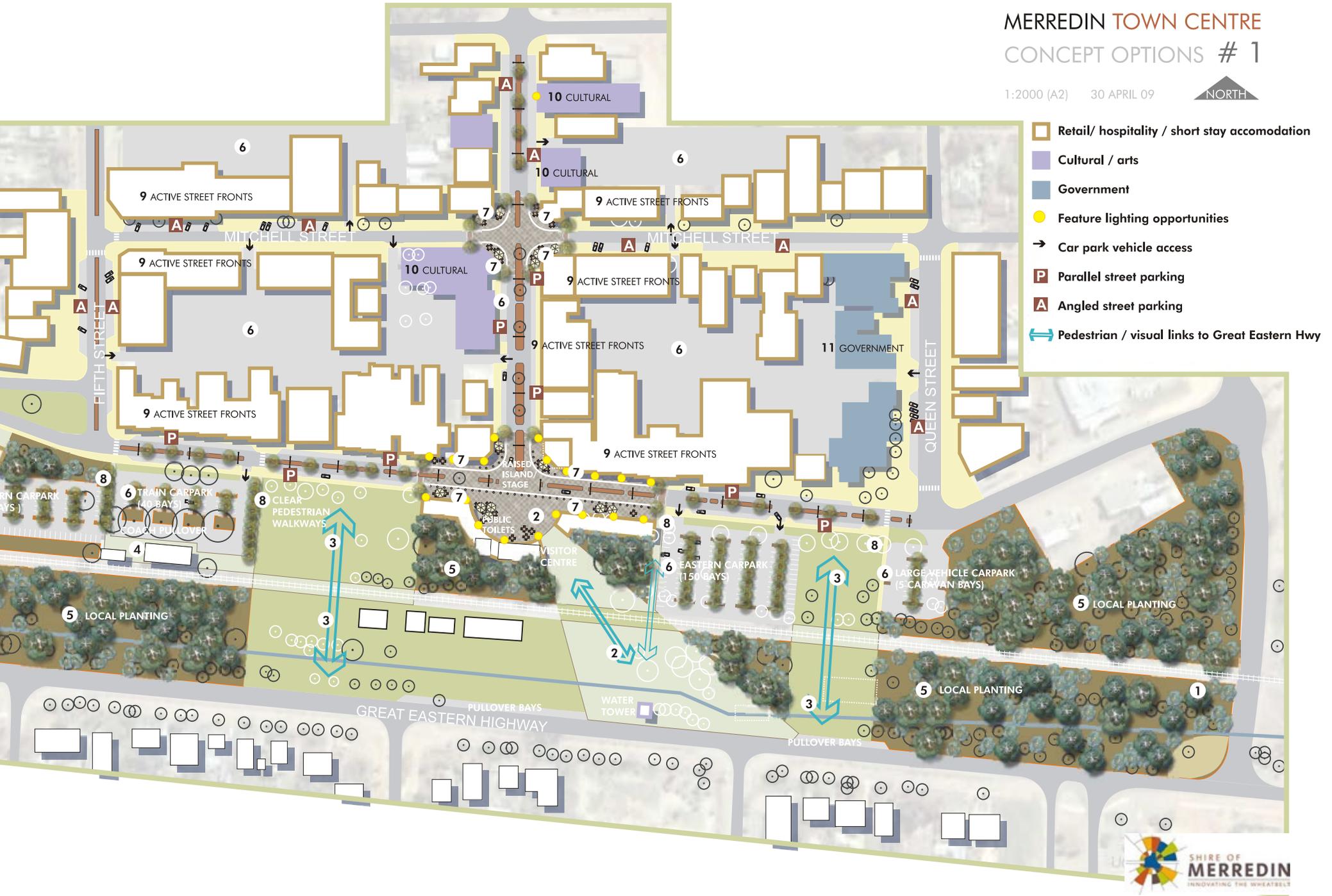
7 - MEETING SPACES

The meeting area has been consolidated to the point of arrival intersection as well as the intersection further along Bates



MERREDIN TOWN CENTRE CONCEPT OPTIONS # 1

1:2000 (A2) 30 APRIL 09



- Retail/ hospitality / short stay accomodation
- Cultural / arts
- Government
- Feature lighting opportunities
- Car park vehicle access
- P Parallel street parking
- A Angled street parking
- Pedestrian / visual links to Great Eastern Hwy

Street. This could potentially have a different pavement treatment, seating, interpretation and widened pathways.

8 - WALKWAYS

Wide pedestrian walkways have been provided from each main car park with clear crosswalks crossing Barrack Street.

9 - RETAIL / HOSPITALITY

ACCOMMODATION

New retail and hospitality should be encouraged emanating from the point of arrival, centre of town, along Barrack and Bates street. It is recommended that all new development within the have zero lot setback, parking to the rear, open and active street address i.e. no blank walls, use of windows etc. All available space should be utilised exploring fresh ideas in retail, alfresco dining, accommodation etc..

10 - CULTURAL / ARTS

A new library centre built at New fields car park would help to create a cultural precinct along Bates Street linking with

Cummins theatre. Another building that could be used for cultural purposes is the old military museum.

11 - GOVERNMENT

Government offices and buildings to be consolidated along Queen Street to create a government buildings precinct. All new buildings to address the street with immediate setback. Keeping these land uses in town enlivens the space and allows staff to use cafes and shops during work hours.



4.2 OPTION TWO

1 - ENTRY

EXISTING

An immediate upgrade could be the removal of the derelict buildings across the highway.

FUTURE

Future investigations into an entry feature could include a landscape treatment that provides a contrast to the dense local planting behind and displays the regions unique flora or agricultural produce e.g. A wheat clearing is a simple statement on Merredin as an agricultural centre in the Wheat belt.

TOURIST ROUTE

Potential to guide tourists into town by means of a tourist route turn off further up the road leading vehicles along the old highway entry.

2 - POINT OF ARRIVAL

CENTRAL ISLAND

The centre of town is heightened by a raised central island and the use of contrasting pavement. Provides a town stage for celebration and market

occasions etc.

PLAZA

The plaza area is defined by an paved break in the open lawn area of the linear park. Denoting a place of importance it not only opens views and connects the highway to the town centre but also makes space available for events and interpretation.

PLANTING

Large feature trees line the plaza area creating a formal event space. These trees define and shade the perimeter of the plaza and direct views to the water tower. Opportunity for families in the area to 'adopt' a tree and label with a family plaque.

WATER TOWER

The water tower becomes a feature in full view from the highway and the town centre.

EXISTING FACILITIES

The public toilets are to be incorporated into the plaza area with the visitor centre to be eventually removed during staged works.

OLD RAILWAY STATION

The old railway station to be renovated as a focal point feature and historical reference point from Bates Street and the highway. The buildings will be multi-purpose incorporating the visitor centre, museum and potentially community activities such as men's shed.

3 - PARKLAND

LINEAR PARK

A continuous network of open lawn spaces, connecting with existing parks (e.g.. Apex and Roy Little) The linear park places importance on open space and makes the parkland a defining feature of Merredin town centre.

Opportunities for rest areas, kick-about, interpretation and community artwork.

4 - TRAIN STATION

ACCESS

Pedestrian area and walkways to Barrack Street provide visitors with clear direction to the town centre. Parking close by allows for easy pick up and drop off.

PLANTING

The Large fig trees are retained as a feature at the station. The remaining area is planted out with endemic plants and trees to provide localised character and sense of place upon arrival.

5 - LOCAL PLANTING

Large tracts of local planting (Gimlets and Salmon Gums) provide a contrast to the open lawn areas and urban spaces. The endemic flora provides a local sense of place, may attract local bird life such as the black cockatoo, and also acts as a feature for tourists to the area. Collecting seed, growing local plants and planting this area out, could potentially be a community driven process and event.

6 - PARKING

WESTERN CAR PARK

Consolidated parking for the train station, café, Apex park and the western end of Barrack Street. Two entry points and large pull over bays under the fig trees allow for coach transport.

EASTERN CAR PARK

Consolidated car park provides parking for the eastern end of town, in particular for the supermarket. The eastern most end accommodates for larger vehicles with trailers or caravans.

STREET PARKING

All street parking along Barrack, Bates, Mitchell, Fifth and Queen Streets is parallel parking. Angled parking is now removed from the Town .

STAFF AND EXCESS SHOP PARKING

Any excess shop parking or staff parking should be provided at the rear of buildings. No large car parks haven street frontages as this will only stop pedestrian flow along the street.

PULLOVER BAYS

Parking bays along the highway allow cars to stop, explore and take advantage of any photo opportunities.

7 - MEETING SPACES

Selected shop front areas along Bates and Barrack Street are to be opened up for alfresco dining or rest areas.

This could potentially have a different pavement treatment, seating, Interpretation and widened pathways.

8 - WALKWAYS

Wide pedestrian walkways have been provided from each main car park with clear crosswalks crossing Barrack Street.

9 - RETAIL / HOSPITALITY

ACCOMMODATION

New retail and hospitality should be encouraged emanating from the point of arrival, centre of town, along Barrack and Bates Streets. It is recommended that all new development within the have zero lot setback, parking to the rear, open and active street address i.e. no blank walls, use of windows etc. All available space should be utilised exploring fresh ideas in retail, alfresco dining, accommodation etc.

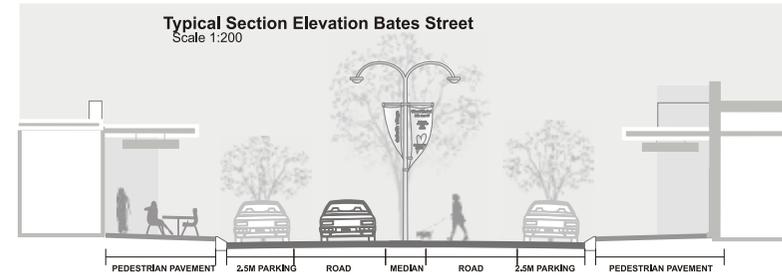
10 - CULTURAL / ARTS

This precinct is connected to Cummins Theatre and the old Town Hall. It would be expected that a new library and possible

cultural / arts building would be combined in this area.

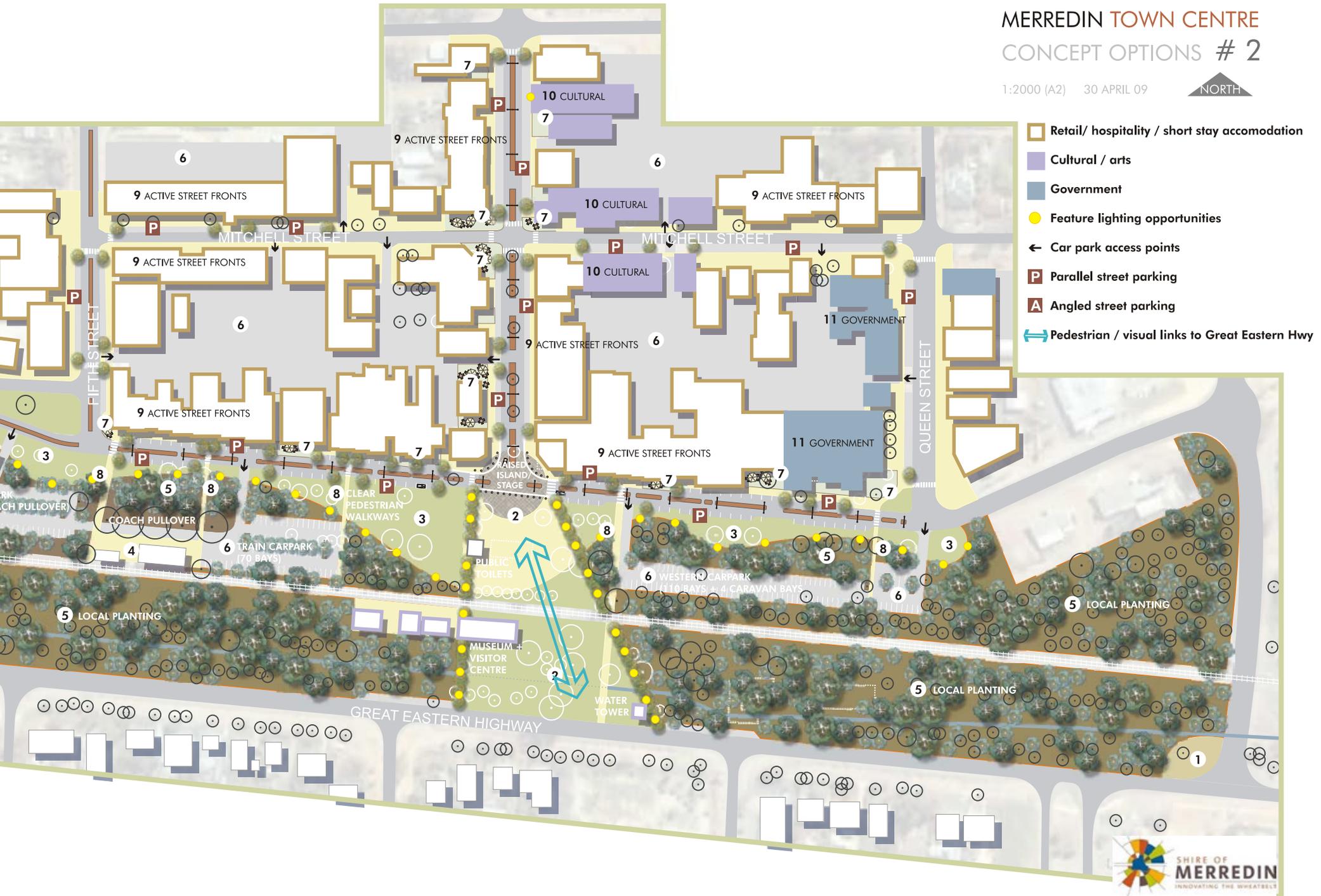
11 - GOVERNMENT

Government offices and buildings to be consolidated along Queen Street to create a government buildings precinct. All new buildings to address the street with immediate setback. Keeping these land uses in town enlivens the space and allows staff to use cafes and shops during work hours.



MERREDIN TOWN CENTRE CONCEPT OPTIONS # 2

1:2000 (A2) 30 APRIL 09



- Retail/ hospitality / short stay accomodation
- Cultural / arts
- Government
- Feature lighting opportunities
- Car park access points
- P Parallel street parking
- A Angled street parking
- ↔ Pedestrian / visual links to Great Eastern Hwy

4.3 OPTION THREE

1 - ENTRY

EXISTING

An immediate upgrade could be the removal of the derelict buildings across the highway.

FUTURE

Future investigations into an entry feature could include a landscape treatment that provides a contrast to the dense local planting behind and displays the regions unique flora or agricultural produce e.g. A wheat clearing is a simple statement on Merredin as an agricultural centre in the Wheat belt.

TOURIST ROUTE

Potential to guide tourists into town by means of a tourist route turn off further up the road leading vehicles along the old highway entry.

3 - POINT OF ARRIVAL

CENTRAL ISLAND

A heightened Town Centre includes part of a raised central promenade and the use of contrasting pavement. This formal urban space could be the

location for markets and other events and include interpretation and artwork. The promenade extends through to Great Eastern Highway linking views and access.

PLANTING

Large feature trees line the promenade creating a formal event space.

EXISTING FACILITIES

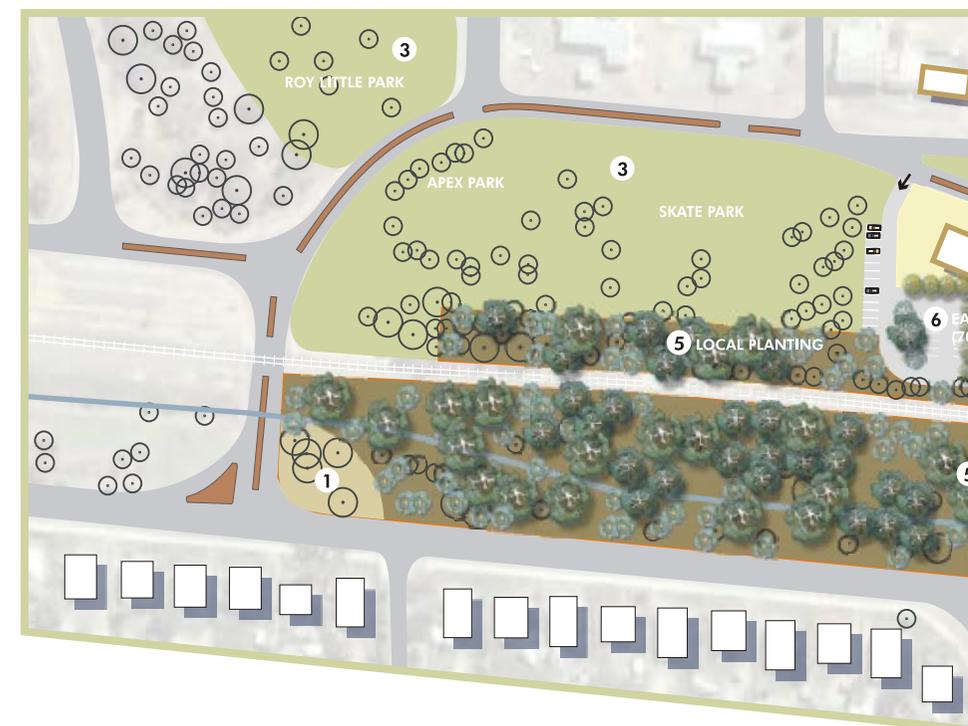
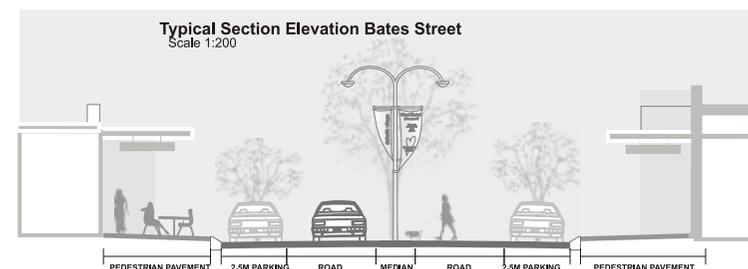
Public toilets to be incorporated into the plaza area with the visitor centre removed during staged work.

OLD RAILWAY STATION

The old railway station to be renovated as a focal point feature and historical reference point from Bates Street and the highway. The buildings will be multi-purpose incorporating the visitor centre, museum and potentially community activities such as men's shed.

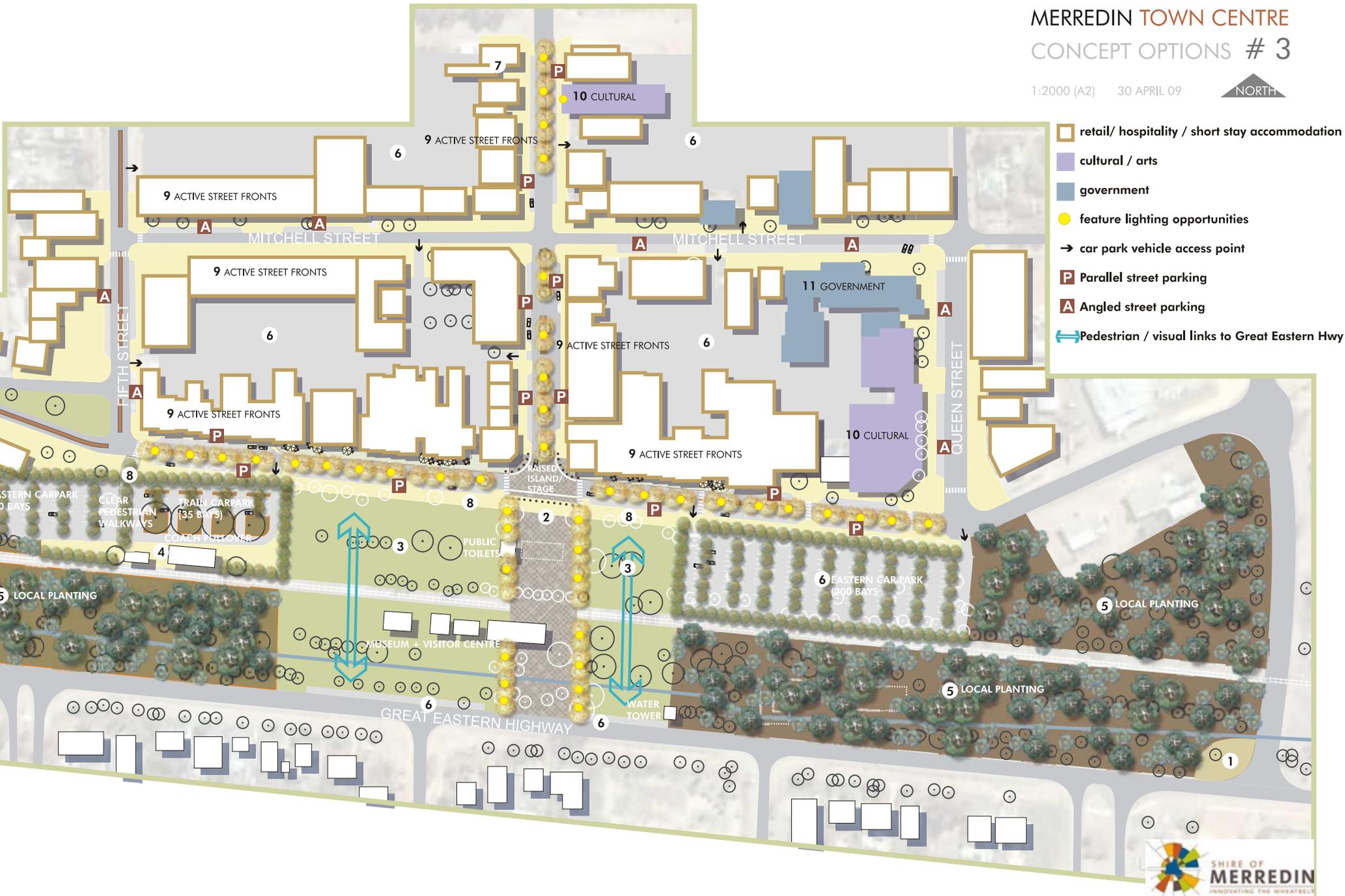
4 - PARKLAND

Two large areas of open space create a large central park area around the point of arrival. This allows large events to 'spill' out into the surrounding area. These areas would be well maintained



MERREDIN TOWN CENTRE CONCEPT OPTIONS # 3

1:2000 (A2) 30 APRIL 09



- retail/ hospitality / short stay accommodation
- cultural / arts
- government
- feature lighting opportunities
- car park vehicle access point
- P Parallel street parking
- A Angled street parking
- ↔ Pedestrian / visual links to Great Eastern Hwy

parkland offering opportunity for public art, community projects, interpretation and general rest areas, kick-about and picnic facilities.

WATER TOWER

The water tower becomes a feature of the eastern parkland area, in full view from the highway and the town centre .

4 - TRAIN STATION

ACCESS

Pedestrian area and walkways to Barrack Street provide visitors with clear direction to the town centre. Parking close by allows for easy pick up and drop off.

PLANTING

The Large fig trees are retained as a feature at the station. A backdrop of local planting on the south side of the line provides a local sense of place at a major point of arrival and departure. Meanwhile, trees chosen for their shade and shape are densely planted throughout the car park areas for amenity.

5 - LOCAL PLANTING

Large tracts of local planting (Gimlets and Salmon Gums) provide a contrast to the open lawn areas and urban spaces. The endemic flora provides a local sense of place, may attract local bird life such as the black cockatoo, and also acts as a feature for tourists to the area.

6 - PARKING

WESTERN CAR PARK

Consolidated parking for the train station, café, Apex park and the western end of Barrack Street. Two entry points and large pull over bays under the fig trees allow for coach transport.

EASTERN CAR PARK

Consolidated car park provides parking for the eastern end of town, in particular the supermarket. The car bays are wider than a standard bay and have periodic drive through bays catering for vehicles with a trailer or caravan.

STREET PARKING

Parallel parking spaces close to the town centre along the northern verge of Barrack Street are removed and replaced

by a wider pedestrian walkway. Parallel parking bays are provided at the ends of Barrack Street and Bates Street, whilst Mitchell, Fifth and Queen Streets all have angled parking.

STAFF AND EXCESS SHOP PARKING

Any excess shop parking or staff parking should be provided at the rear of buildings. Do not locate large car parks on street frontages as this will only stop pedestrian flow along the street.

PULLOVER BAYS

Parking bays along the highway allow cars to stop, explore and take advantage of any photo opportunities.

7 - MEETING SPACES

The centrally located parallel parking along Barrack Street is to be filled with paving to provide large pedestrian walkways, including alfresco and meeting areas.

8 - WALKWAYS

The wide pedestrian walkways afforded throughout this design create legible and clear pedestrian spaces for both the

locals and visitors.

9 - RETAIL / HOSPITALITY/ ACCOMMODATION

New retail and hospitality should be encouraged emanating from the point of arrival, centre of town, along Barrack and Bates Streets. It is recommended that all new development within the have zero lot setback, parking to the rear, open and active street address i.e. no blank walls, use of windows etc. All available space should be utilised exploring fresh ideas in retail, alfresco dining, accommodation etc.

10 - CULTURAL / ARTS

The Culture and Arts centre is placed on the corner of Barrack and Queen street referencing the opportunity to build a new complex in the area that includes the library.

11 - GOVERNMENT

Government offices and buildings to be consolidated along Queen and Mitchell street, noting a zero lot setback and parking on street or to the rear. Keeping these land uses in town enlivens the space and allows staff to use cafes and shops during work hours.



5.0 PHASE FOUR: DRAFT CONCEPT



The Draft Concept Phase included linking together the preferred design driver (theming) elements from the Exploration Phase, as follows:

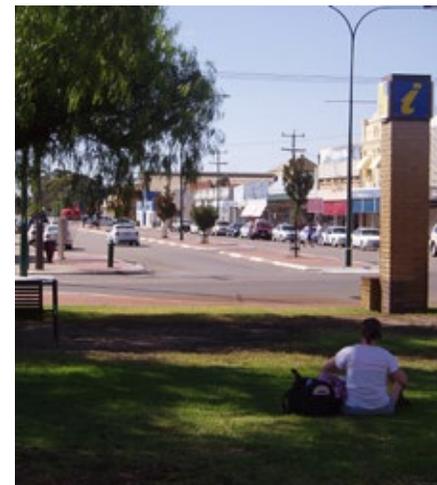
5.1 POINT OF ARRIVAL

- Paving denotes point of significance
- Slightly raised island / Road level same as the lawn area / Acts as a traffic calming device
- Raised island forms a stage area
- Opportunity to close the road for larger events, making this area available for performance/ pedestrians
- Potential for indigenous interpretation to be incorporated into the raised island, as the point of arrival and referencing the dance traditions within the area
- Partnered with the large open lawn area and plaza, views are opened to significant locations (e.g. railway museum, post office and town centre from the highway)

- The lawn caters for everyday use and various events such as markets, busking, concerts and dances etc.
- This space creates a reference point that both locals and tourists can navigate from
- Shelters, bike racks, BBQs and picnic facilities
- Maintained as an area where Merredin can show itself off through art, community activities and landscape
- Staged removal of Visitor centre
- Incorporate Visitor Centre within the Railway Museum
- Future plans could see the public toilets relocated out of the open lawn area (to a position still at the point of arrival but perhaps out of the line of sight to the side and back)

OPTION

Build a new Visitor centre west of the public toilets to create a more intimate centre, maintaining views to the Railway Museum and also tourists do not have to cross the railway line from the Town centre



5.2 PIONEER PLAZA

- Provides visual connections from Highway to Town
- Clear pedestrian access
- Location for community events / activities
- Potential tourist node
- Opportunity for interpretation tying in with the heritage Railway buildings as a future Museum (and potentially a Visitor Centre)
- Low maintenance ground treatment suitable for various activities and access (e.g. granitic sand)

5.3 PRECINCTS

COMMERCIAL

- Barrack and Bates Street
- Building on past and present town planning
- Make these streets activated as the predominant shopping strips

CULTURAL

- Cummins Theatre and the Old Town Hall as focal points and boundaries to a corner precinct for the culture and the arts
- Corner Bates and Mitchell Streets
- Infill development as required would occur here
- Potential to include a new Library facility (not limiting its facilities to conventional books only)

GOVERNMENT

- Building upon existing buildings (e.g. library)
- Within government owned land
- Precinct along Queen Street
- Staff can use the towns shops and cafes during work hours

MIXED USE

- Located on secondary streets (Mitchell, Queen, Fifth Streets)
- Offices, tourist accommodation, larger warehouses etc.
- Less likely to activate the street and are therefore located off the main shopping strips.

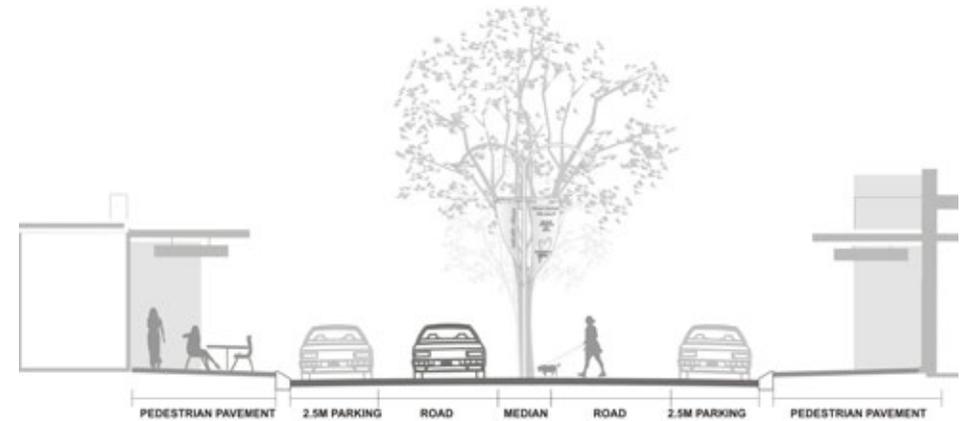
RESIDENTIAL

- Infill back into the outskirts of the with high density residences will encourage activation and security within the town centre.
- Ideal for people who do not wish to or cannot use a car to get into town.

LARGE FORMAT/ WAREHOUSE

- Located on the outer as they comparatively do little to activate the street and require space for larger vehicles to access them.





Typical SectionAA Elevation Bates Street

Scale 1:200

COMMERCIAL	920 Lm
CULTURAL	160 Lm
GOVERNMENT	185 Lm
MIXED USE	740 Lm
RESIDENTIAL	330 Lm (within study area boundary)
LARGE FORMAT/ WAREHOUSE	300 Lm (within study area boundary)

Buildings on Barrack, Bates, Mitchell, Fifth and Queen Streets should maintain zero lot setback and address the street.

Retaining awnings along walkways provides increased amenity and shade for pedestrians.

5.4 SOUTH HEDLAND RETAIL CASE STUDY

Case studies were explored that discussed the benefits of each preferred design driver, including the benefit of centralising retail opportunities within a focused area to complement a friendly walkable environment.



EXISTING

- Internal shopping centre
- Dominated by two large companies
- Small businesses struggling



EXPECTED

- Upgraded was going to fill the loop road



PROPOSED

- Retail planner / analyst (Greg Davies, Tactics 4) advised for heavily reduced retail area for population numbers

	SOUTH HEDLAND	MERREDIN
Population	16,000 (unknown catchment)	3,246 (approx. 20,000 catchment)
Commercial	800 Lm	920 Lm

5.5 PARKING

LARGE CARPARKS

- Same location
- Safe vehicle and pedestrian routes
- Excess bays for various locations (e.g. Apex, west town, IGA, east town)
- Consolidate bays
- Clear access
- Clear marked bays
- Shaded
- 120m walking distance between car parks

STREET PARKING

- Maintain existing bays on street

PULLOVER BAYS

- 12 bays along Great Eastern Highway for passing traffic

CAR BAY SIZES

- Maintain large car bay sizes for large vehicles
- Current size: 4.5 - 5.5 X 3m
- Proposed size: 5.5 X 3m



CARAVAN / LARGE VEHICLE PARKING

- Diamond tree planters with flush kerbs allow drive through bays for trailers, caravans etc.

WESTERN CARPARK

	Existing	Proposed
Area	16,175m ²	16,200m ²
No. of bays	248	300

EASTERN CARPARK

	Existing	Proposed
Area	8,157m ²	16,200m ²
No. of bays	136	200

5.6 OPEN SPACE

LINEAR PARK

- Referencing an existing element
- Softens Barrack streetscape
- Existing trees can be retained (as they reach the end of their natural life they can be removed and not replaced opening views to the continuous parkland)
- Clearly connects existing parks to the Town Centre

WALKWAY

- Parkland bound by a sinuous walkway
- Entire length of the linear park
- Clear walkway to existing parks
- Clear walkway and different experience of Barrack Street
- Circuit trail from plaza
- Lined by night lighting along Barrack Street provides an attractive nightscape and also enables people using local restaurants and entertainment to enjoy an evening stroll.
- Opportunity for local stories to be



interpreted along walkway

GARDENS

- Backdrop to the linear park
- High quality gardens
- Endorses an attractive streetscape
- 'Show off' endemic plants and exotic species that relate to Merredin's history
- Grouped tree planting provides shade and amenity for walkers and rest areas

	Apex Park	South of Barrack	Front of Visitor Centre	Pioneer Park
Existing Area	10,300m ²	3,500m ²	1,200m ²	8,000m ²
Proposed Area	10,200m ²	6,500m ²	7,000m ²	9,500m ²

- Keeping these trees setback from Barrack street provides clear open views of the park area and avoids potential conflict with power lines, preventing hazards and allowing the trees to mature with healthy and good form

- Local rocks can also form part of the display within the gardens.

5.7 LOCAL PLANTING

- A contrast to open lawn areas
- May attract local birdlife such as the black cockatoo
- Reflects a local sense of place
- Feature for tourists
- Defined and contained area of local planting
- Unique green belt along GE Highway, revealing views into the town at the plaza
- Potential for the walk track that lines the large tract of local planting to have interpretation explaining different types of local plant life and its significance to the indigenous, settlers, farmers and local fauna



	Existing	Proposed
Area	77,600m ²	63,600m ²

5.8 PUBLIC ART / COMMUNITY PROJECTS / INTERPRETATION

PROVIDES OPPORTUNITY FOR

- Community ownership
- Visual narrative
- Fun and play in the landscape
- Historical references
- Connection to cultural landscape
- Reference farming history and industry in town centre
- Education
- Awareness

POTENTIAL WORKS INCLUDE

- Community garden (as a demonstration garden for the tourist trade)
- Rare wildflower garden
- Indigenous interpretation
- Military and agricultural interpretation
- Historical interpretation
- Botanical interpretation
- School artwork/ projects
- Memorials (e.g. Nurse memorial

garden)

Locating community driven activities / events / works within the also:

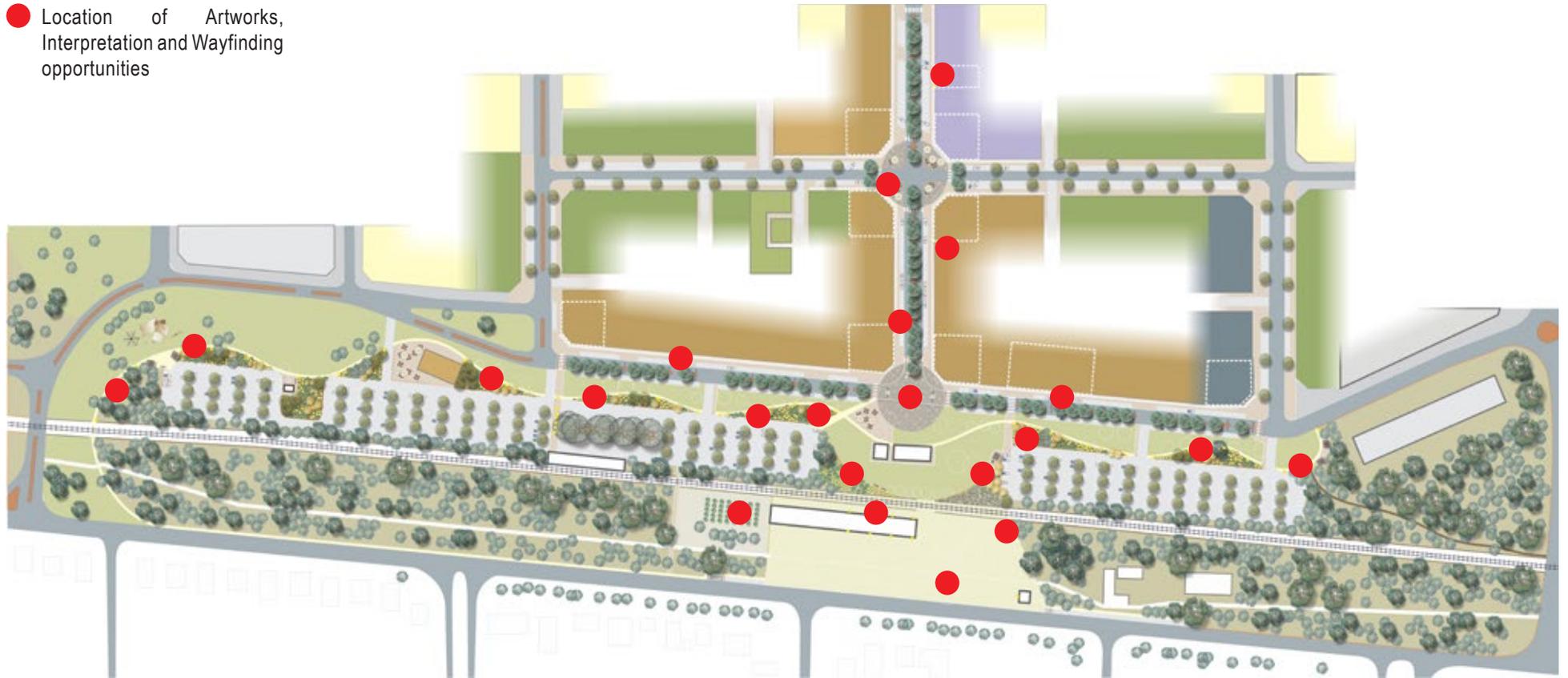
- Ensures they are accessible
- Increased surveillance
- Wider audience (tourist and local)
- • Greater appreciation
- • Daily interaction

POTENTIAL LOCATIONS INCLUDE

- Linear park
- Meandering walkway
- Point of arrival
- Pioneer plaza
- Barrack and Bates Streets pathways
- Barrack and Bates Streets shop fronts
- Street furniture
- Railway Museum



● Location of Artworks, Interpretation and Wayfinding opportunities



6.0 FINAL CONCEPT



A final concept was developed after receiving collated input from the draft concept plan.

The following documents the spatial, elements of the final preferred Merredin Business Centre Concept Plan.

A proposed implementation staging plan documents the preferred priority of works to be undertaken over time. An estimate of cost accompanied the report for the Shire of Merredin's information.

LINEAR PARK

- Referencing an existing element
- Softens Barrack Street scape
- Existing street trees may be retained. (As they reach the end of their natural life, they should be removed and not replaced, opening views to the continuous parkland)

WALKWAY

- Parkland bound by a sinuous walkway
- Entire length of the linear park
- Connects existing parks to the Town

- Circuit trail from plaza
- Along Barrack Street the path may be lined by night lighting, providing a place for patrons using local restaurants and entertainment to enjoy an evening stroll
- Opportunity for local stories to be interpreted along the walkway

GARDENS

- Backdrop to the linear park
- High quality gardens
- Endorses an attractive streetscape
- 'Show off' local plants and exotic species that relate to Merredin's history
- Grouped tree planting provides shade amenity for walkers and rest areas
- Keeping trees set back from Barrack Street avoids potential conflict with powerlines, preventing hazards and allowing the trees to mature with healthy and good form
- Local rocks can also form part of the display within the gardens

POINT OF ARRIVAL

- Paving denotes point of significance
- Slightly raised island
- Road level same as the lawn area.
- Acts as a traffic calming device
- The semi circle that extends into the parkland forms stage area
- Opportunity to close the intersection for large events, making this area available for performance / pedestrians
- As the point of arrival there is potential for indigenous interpretation to be incorporated into its design and construction referencing the strong dance traditions within the area
- Partners with the large open lawn area to open views to significant locations such as the old railway museum, the post office and the town centre from the highway
- The lawn caters for an everyday kick about and various events such

as markets, busking, concerts and dances etc.

- This point of arrival space creates a reference point that both locals and tourists can navigate from. To be maintained as an area where Merredin can show itself off through art, community activities and landscape

OPEN PLAZA

- Provides views
- Clear pedestrian access to town.
- Location for community events / activities
- Potential tourist node
- Opportunity for interpretation tying in with the Heritage Railway Buildings as a future Visitor Centre and Museum

COMMUNITY GARDEN

- Potential for a community garden
- Central location
- Accessible
- Potential to use it as a demonstration garden for the tourist trade

NURSE MEMORIAL GARDEN

- Consolidating roses
- Central location to be appreciated and maintained
- Low plantings of rose bushes maintains views to the heritage railway building

WATER TOWER

- The plaza opens views to the water tower making it a feature within this landscape. Uplighting will make it a feature at night

BARRACK AND BATES STREETScape

- Predominantly Retail and inclusive of the cultural precinct
- Car bays within the raised island to be removed to relieve traffic conflict at this intersection
- Car bays supplemented by additional parking elsewhere
- Dense tree planting along the median strip only, delineating the 2 main shopping streets
- No trees on the edge of the road to avoid conflict between trees and

powerlines, thus ensuring strong tree growth and good form

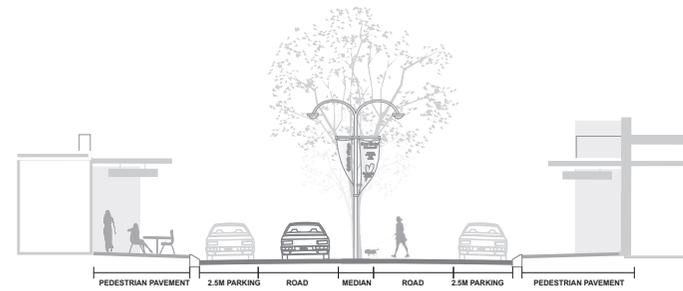
- Trees away from buildings allows signage and old buildings to be highly visible
- Buildings should maintain immediate setback that addresses the street
- All space should be utilised exploring new and innovative urban designs
- ALFRESCO DINING

The Bates Street and Mitchell Street intersection provides an opportunity to draw people up Bates Street through alfresco dining on the intersection corners.

- Supported by surrounding business staff and tourists during the day
- Close proximity to the cultural / arts precinct creates a hub for evening dining

MITCHELL, FIFTH AND QUEEN STREETS Scape

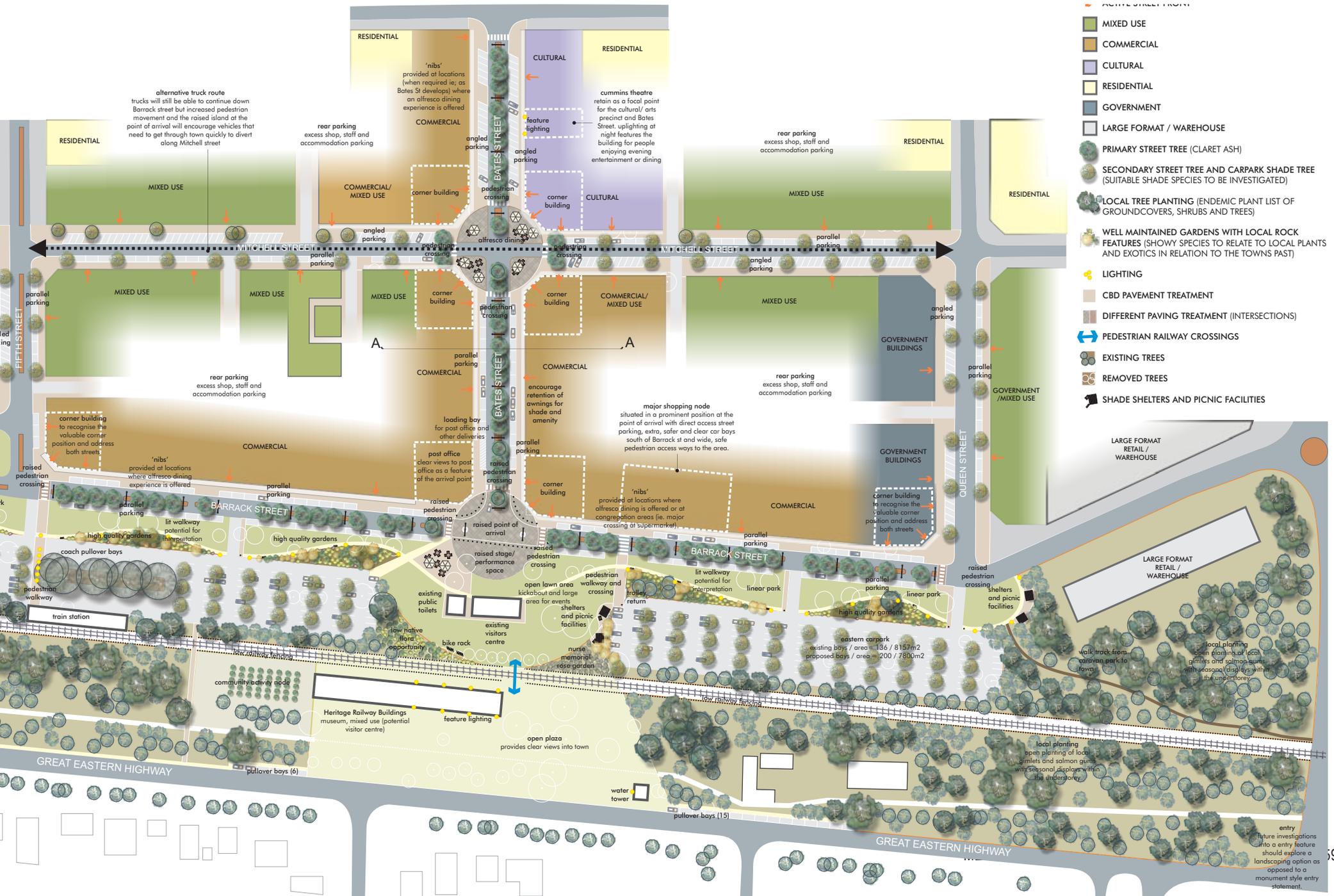
- Existing car bays to be retained
- Shade trees along the outer edge of the road to provide shade for the parked vehicles and pedestrian



Typical Section AA Elevation Bates Street
Scale 1:200



• conceptual only - no survey information was used to produce this drawing
• base information from scaled aerial
• all detail work to be undertaken following site survey



- ASHLEY STREET FRONT
- MIXED USE
- COMMERCIAL
- CULTURAL
- RESIDENTIAL
- GOVERNMENT
- LARGE FORMAT / WAREHOUSE
- PRIMARY STREET TREE (CLARET ASH)
- SECONDARY STREET TREE AND CARPARK SHADE TREE (SUITABLE SHADE SPECIES TO BE INVESTIGATED)
- LOCAL TREE PLANTING (ENDEMIC PLANT LIST OF GROUNDCOVERS, SHRUBS AND TREES)
- WELL MAINTAINED GARDENS WITH LOCAL ROCK FEATURES (SHOWY SPECIES TO RELATE TO LOCAL PLANTS AND EXOTICS IN RELATION TO THE TOWNS PAST)
- LIGHTING
- CBD PAVEMENT TREATMENT
- DIFFERENT PAVING TREATMENT (INTERSECTIONS)
- ↔ PEDESTRIAN RAILWAY CROSSINGS
- EXISTING TREES
- REMOVED TREES
- SHADE SHELTERS AND PICNIC FACILITIES

- traffic
- The change in street scape denotes a secondary street
- Predominantly mixed use but inclusive of commercial and government land use
- Immediate setback that addresses the street should be maintained
- All development should explore new and innovative ways of utilising space within these streets

OLD RAILWAY BUILDINGS

- Open views to the heritage buildings
- Renovate and restore
- Creates a focal point for vehicle and pedestrian traffic
- A strong and attractive visual clue to Merredin's history
- For an active centre and efficient staffing, the buildings should be multi-use incorporating the museum and making space available for community groups such as men's shed, art groups and community garden works etc.

- Uplighting creates a feature and focal point at night for passing traffic
- LOCAL PLANTING
 - Potential for the walk track that lines the large tract of local planting to have interpretation explaining the different types of local plant life and its significance to the indigenous, settlers, farmers and local fauna
 - A contrast to the open lawn areas
 - May attract local bird life such as the Black cockatoo
 - Reflects a local sense of place
 - Feature for tourists

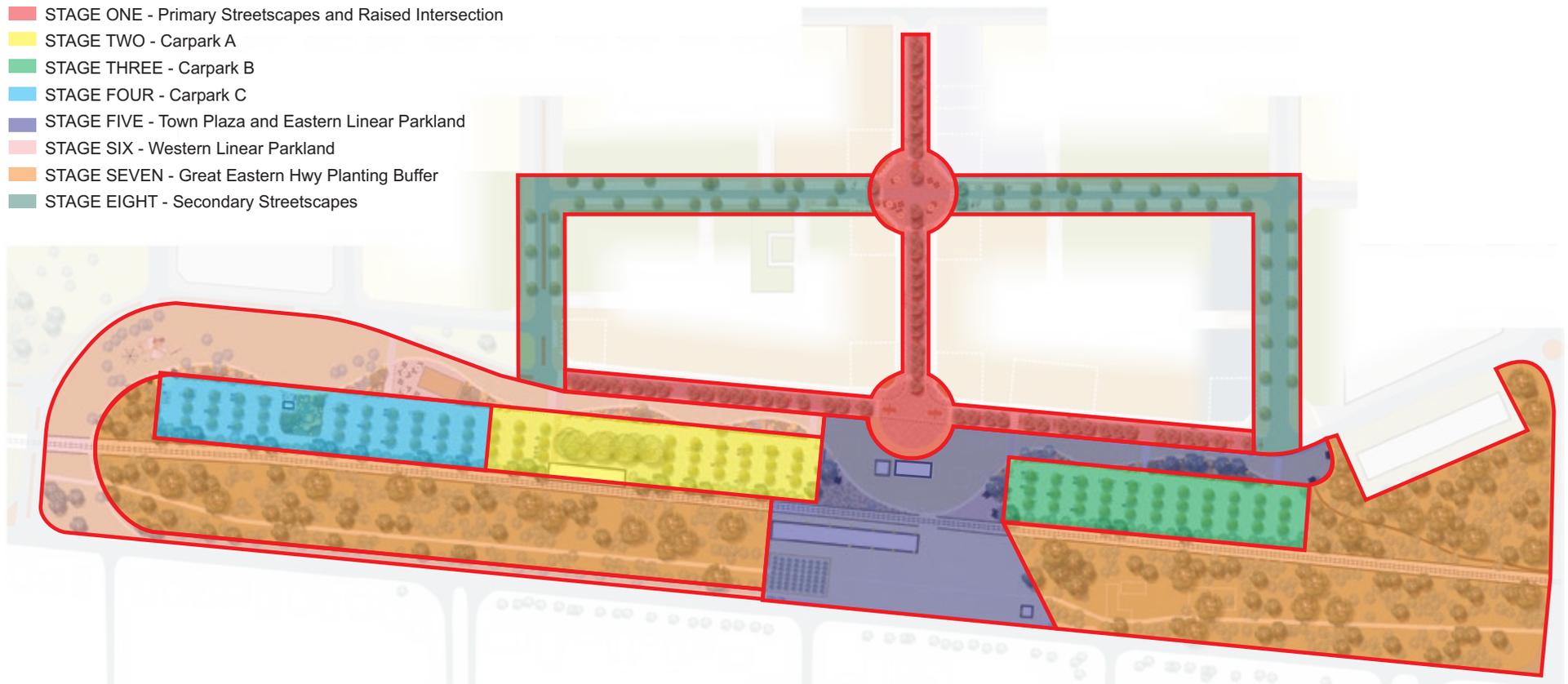
REST AREAS/ PICNIC FACILITIES

- The meandering parkland opens up to provide a variety of spaces for active and passive recreation



6.1 STAGING

- STAGE ONE - Primary Streetscapes and Raised Intersection
- STAGE TWO - Carpark A
- STAGE THREE - Carpark B
- STAGE FOUR - Carpark C
- STAGE FIVE - Town Plaza and Eastern Linear Parkland
- STAGE SIX - Western Linear Parkland
- STAGE SEVEN - Great Eastern Hwy Planting Buffer
- STAGE EIGHT - Secondary Streetscapes



6.2 TABLE OF CONSTRUCTION MILESTONES IN DETAIL

Milestones	Sub-Milestones	Details
1. Inception and Logistics	1.1 Inception	<ul style="list-style-type: none"> Project manager onboard the team with the lead contractor
	1.2 Logistics	<ul style="list-style-type: none"> Contractors assemble plant, materials, labour
2. Primary Streetscapes and Raised Intersection	2.1 Site Preliminaries	<ul style="list-style-type: none"> Prepare pedestrian pavement and road area for re-surfacing Earthworks and Fine Trimming Raised Intersection Mobilisation and Insurances
	2.2 Hard Landscape	<ul style="list-style-type: none"> Pavement to 2 x intersection including adjacent Town Plaza Repair of adjacent concrete pedestrian pavement Supply and installation of bollards at 2 x intersections Supply and installation of raised crosswalks (requires civil drainage plan) Painted Crosswalks at Primary interesections Supply and installation of tree diamond cutouts and prepare planter
	2.3 Soft Landscape	<ul style="list-style-type: none"> Supply 100 ltr Street Trees including Tree Guard, mulch and Deep Watering Tube
3. Carpark A	3.1 Site Preliminaries	<ul style="list-style-type: none"> Cleanup of existing carpark area Removal of existing trees Prune and protect existing trees to be retained Earthworks and fine trimming of carpark Mobilisation and Insurances
	3.2 Hard Landscape	<ul style="list-style-type: none"> Supply and install asphalt pavement to carpark including linework and tree diamonds
	3.3 Soft Landscape	<ul style="list-style-type: none"> Supply 100 ltr Street Trees including Tree Guard, mulch and Deep Watering Tube

Milestones	Sub-Milestones	Details
4. Carpark B	4.1 Site Preliminaries	<ul style="list-style-type: none"> • Cleanup of existing carpark area • Removal of existing trees • Prune and protect existing trees to be retained • Earthworks and fine trimming of carpark • Mobilisation and Insurances
	4.2 Hard Landscape	<ul style="list-style-type: none"> • Supply and install asphalt pavement to carpark including linework and tree diamonds • Supply and installation of lighting (solar)
	4.3 Soft Landscape	<ul style="list-style-type: none"> • Supply 100 ltr Street Trees including Tree Guard, mulch and Deep Watering Tube
5. Carpark C	5.1 Site Preliminaries	<ul style="list-style-type: none"> • Cleanup of existing carpark area • Removal of existing trees • Prune and protect existing trees to be retained • Earthworks and fine trimming of carpark • Mobilisation and Insurances
	5.2 Hard Landscape	<ul style="list-style-type: none"> • Supply and install asphalt pavement to carpark including linework and tree diamonds • Supply and installation of lighting (solar)
	5.3 Soft Landscape	<ul style="list-style-type: none"> • Supply 100 ltr Street Trees including Tree Guard, mulch and Deep Watering Tube
6. Town Plaza and Eastern Linear Parkland	6.1 Site Preliminaries	<ul style="list-style-type: none"> • Cleanup of parkland and plaza area • Removal of existing trees • Prune and protect existing trees to be retained

Milestones	Sub-Milestones	Details
	6.2 Hard Landscape	<ul style="list-style-type: none"> • Mobilisation and Insurances • Install concrete pads • Install concrete paths • Supply and install compacted granatic sand in Town Plaza (including edging) • Install bollards within parkland • Supply and install feature uplighting (railway museum, water tower, etc.) • Supply and install walkway lighting (solar) • Install Shade Structures • Supply and install bike racks • Supply and install 40 ltr local tree groupings, tied and staked, deep watering tube • Supply and spread mulch to garden beds • Supply and plant tubestock to garden beds
	6.3 Soft Landscape	<ul style="list-style-type: none"> • Supply drip irrigation to garden bed plantings • Supply and install instant turf to open parkland including irrigation • Supply and place local granite feature boulders
7. Western Linear Parkland	7.1 Site Preliminaries	<ul style="list-style-type: none"> • Cleanup of parkland area • Removal of existing trees • Prune and protect existing trees to be retained • Mobilisation and Insurances

Milestones	Sub-Milestones	Details
	7.2 Hard Landscape	<ul style="list-style-type: none"> • Install concrete pads • Install concrete paths • Install bollards within parkland • Supply and install feature uplighting (railway station, etc.) • Supply and install walkway lighting (solar) • Supply and install bike racks
	7.3 Soft Landscape	<ul style="list-style-type: none"> • Supply and install 40 ltr local tree groupings, tied and staked, deep watering tube • Supply and spread mulch to garden beds • Supply and plant tubestock to garden beds • Supply drip irrigation to garden bed plantings • Supply and install instant turf to open parkland including irrigation • Supply and place local granite feature boulders
8. Great Eastern Highway Planting Buffer	8.1 Site Preliminaries	<ul style="list-style-type: none"> • Cleanup of existing buffer area including pruning and removal of existing trees (as required)
	8.2 Hard Landscape	<ul style="list-style-type: none"> • Supply and install compacted gravel bush track
	8.3 Soft Landscape	<ul style="list-style-type: none"> • Community tube stock planting (winter planting)
9. Secondary Streetscapes	9.1 Site Preliminaries	<ul style="list-style-type: none"> • Earthworks, fine trimming and prepare road verges (where required) • Mobilisation and Insurances
	9.2 Hard Landscape	<ul style="list-style-type: none"> • Supply and construct asphalt car bays including kerbing, line marking and tree diamond planters • Supply and construct concrete pedestrian pavement
	9.3 Soft Landscape	<ul style="list-style-type: none"> • Supply 100 ltr Street Trees including Tree Guard, mulch and Deep Watering Tube

7.0 2020 UPDATES

Shire of Merredin Draft Community Vision:

'Merredin is the commercial and cultural heart of the eastern wheatbelt region. A place people are proud to call home and where visitors are always welcome.'

Image: Pioneer Avon Park, York WA

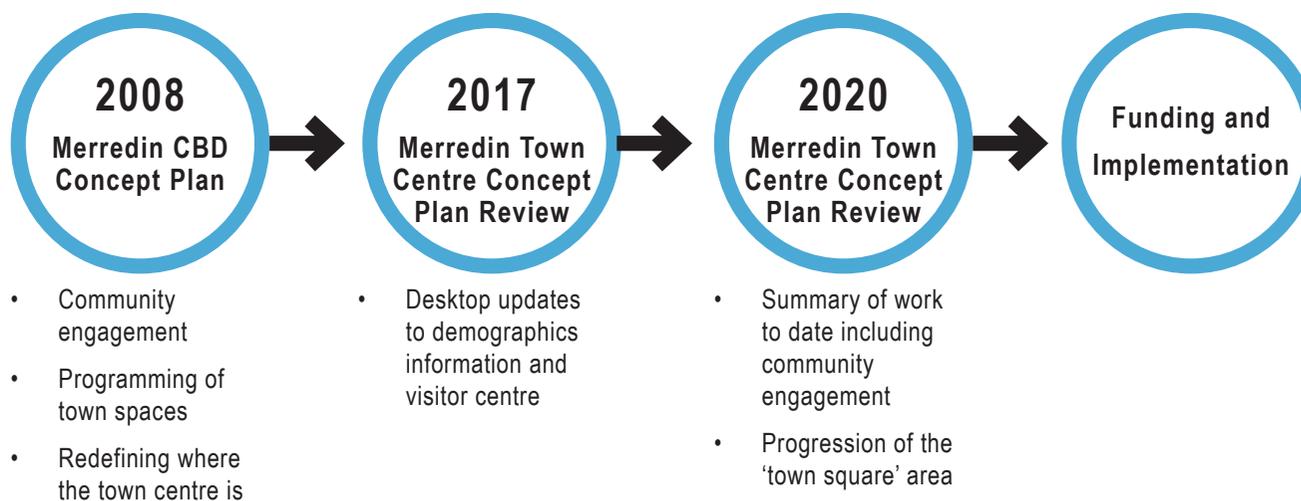
A wide-angle photograph of Pioneer Avon Park in York, WA. The scene is set on a bright, sunny day with a clear blue sky. In the foreground, there's a dirt and gravel area. To the left, a large, curved, orange-brown concrete structure sits on a patch of mulch. In the middle ground, a shelter with a light-colored roof and metal frame stands on a paved area. Several people are sitting on benches under the shelter. To the right, a large, colorful wooden play structure made of thick logs is visible. The background is filled with tall, leafy trees, and a large, mature tree with a thick trunk stands prominently on the right side of the frame.

7.1 INTRODUCTION

Since 2008 the revitalisation of the Merredin town centre has remained a high priority for the community and the Shire. The purpose of the addition of this chapter to the 2017 updates aims to:

- Provide a review of the town centre concept plan;
- Assessment of changes within the town and broader region in regards to demographics and attitudes;
- Summary of community engagement undertaken in 2019/2020; and,
- A clear way forward for the Shire in order to prioritise works and identify potential funding sources.

This chapter builds upon the work undertaken to date and notes the importance of the community engagement undertaken during the development of the first report.



7.2 IMPLEMENTATION SO FAR

The Shire has undertaken upgrades to the railway car park and landscape upgrades adjacent the pedestrian rail crossing to the south.

To move implementation forward for the town centre it may be valuable to identify projects that can be undertaken as part of the Shire's on-going works such as streetscape upgrades, and those which need additional funding and design.



Figure 1. Areas where upgrades have been undertaken since 2008

7.3 DEMOGRAPHICS

The 2016 census data has already been included in the update of demographics contained in this report. Below we note the Estimates Resident Population (ERP) provided each year by the Australian Bureau of Statistics. This is based on trends and the 2016 census, with revisions made after each census, with the next census to be conducted in 2021.

The population estimate for the Shire of Merredin as of the 30th June 2019 is 3,365 a decline of 1.29% from the 2018 ERP. Population growth in Regional WA was -0.01%.

Trends across Australia are currently towards a shift of population from large cities to regional centres due to Covid-19, potentially meaning an increase in people living regionally.

7.4 2019/2020 ENGAGEMENT SUMMARY

Apex Park Play Equipment 2019

Based on selection of off-the-shelf equipment where participants were requested to choose favourite items.

General requests included drink fountains play area for young children/toddlers Popular equipment included a tower structure with multiple slides, flying fox and nest swing.

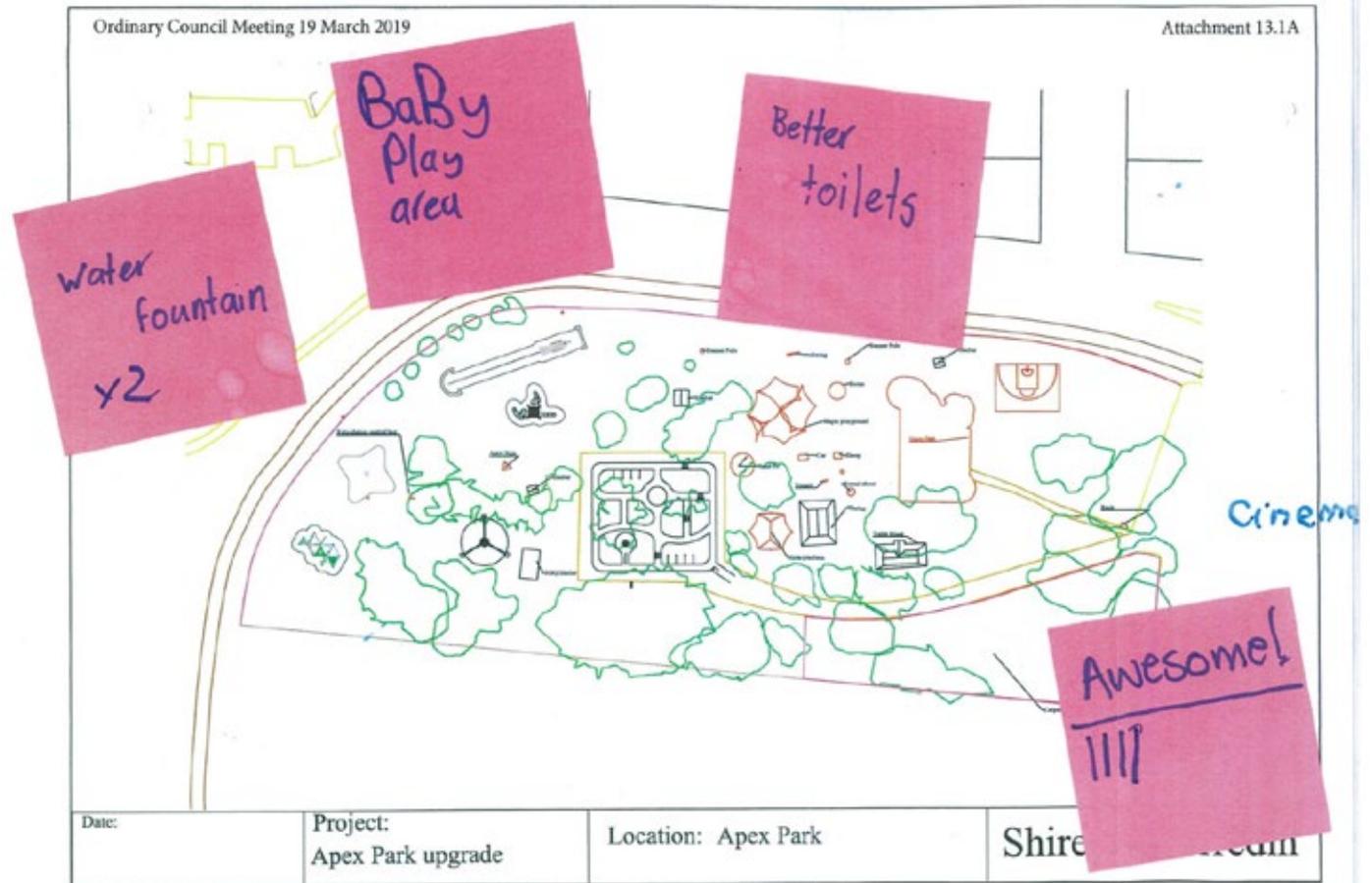


Figure 2. Apex Park engagement

Apex Park Redevelopment Engagement 2020

A series of engagement sessions were undertaken using imagery to determine more aspirational objectives for Apex Park with a focus on play elements. Some key aesthetics and community desires determined from this process include:

- Nature play;
- Key attractor in the form of specialist play items;
- Water;
- Night time use/lighting;
- Toilets; and,
- Shade.

This engagement provides a good starting point for conceptual design of a new playground



Image 2. Learning opportunities



Image 3. Play Elements





Image 4. Materials



Image 6. Facilities



Image 5. Visitor attractions



Image 7. Colours and tone

Strategic Community Plan 2020 Councillor Review

Merredin's Unique Story

Strengths

- Mid point between Perth and Kalgoorlie
- Gateway to landscape history and heritage; rocks and camping, pipeline and railway
- Renewable energy centre
- Warm winter climate
- College and education
- Health and hospitality
- Transport and services hub
- 2,000 vehicles per day pass through Merredin
- Niches / Small scales that could grow
- Aged care industry
- Land availability
- Satellite towns – Muntadgin and Burracoppin
- Multiculturalism derived from water, rail and agriculture
- Active volunteer base

- Theatre/Fine Arts & Cultural Activity
- Sport
- Community Involvement / Reliance on Shire
- Strong community organisations e.g. Men's Shed

Weaknesses

- Stories do not capture return trips

Opportunities

- Opportunity to engage with pre-colonial Indigenous history and make accessible to the community
- Keep old stories while creating shared memories for the future
- Develop tourism and capture more return visitors
- Museum for sharing stories
- Recreational opportunities – bike paths
- Waste – Regional Waste Facility e.g. Tamala Parkland
- Community garden and/or other initiatives that engage the community
- Program public realm to consider seniors

Merredin 2030 Data Review

- Community and Culture, and Economy and Growth are broad concerns held by the community.
- Retail options remain a major concern for the community.
- In regards to the CBD revitalisation Apex Park redevelopment is by far a priority within the community.

TRENDS IN COMMUNITY CONCERNS

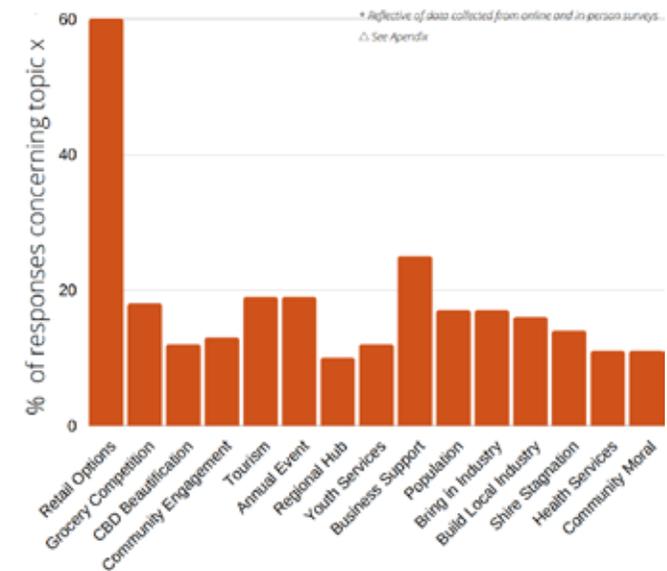


Figure 3. Merredin Data Review Community Concerns excerpt

Broad Distribution of Community Concerns

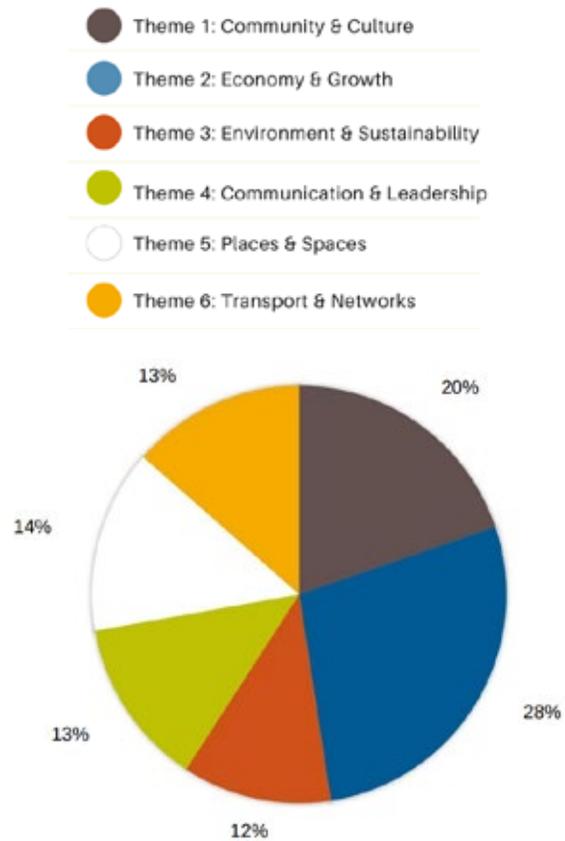


Figure 4. Merredin Data Review Community Concerns excerpt

CBD REDEVELOPMENT PRIORITIZATION

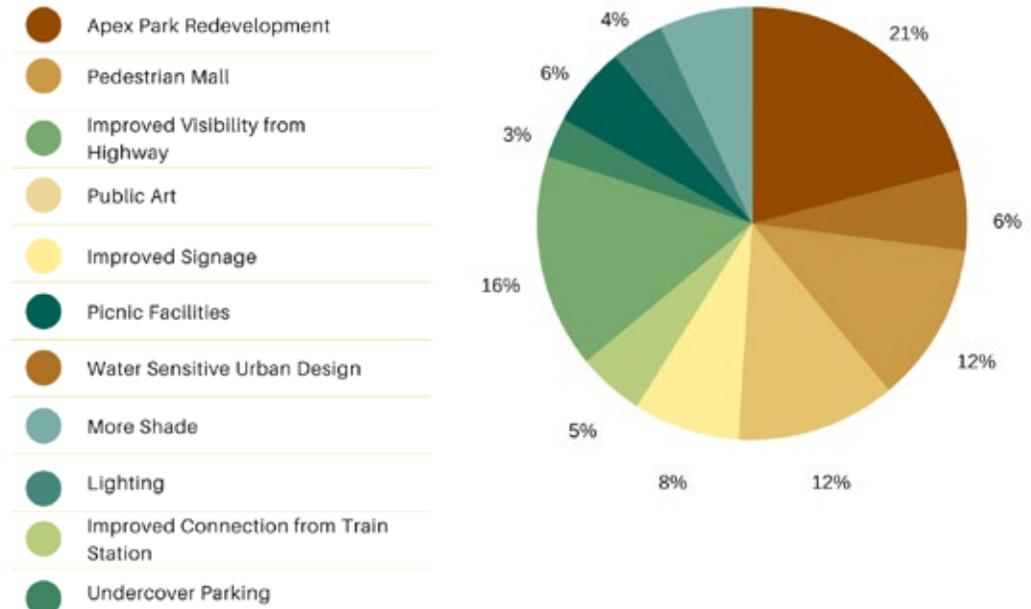


Figure 5. Merredin Data Review CBD Priorities

7.5 PRIORITIES MOVING FORWARDS

Aim: Kick start implementation of the Town Centre Revitalisation.

We believe the desires and concerns of the community has not greatly changed from the development of the original town centre plan as such there should be a focus on implementation.

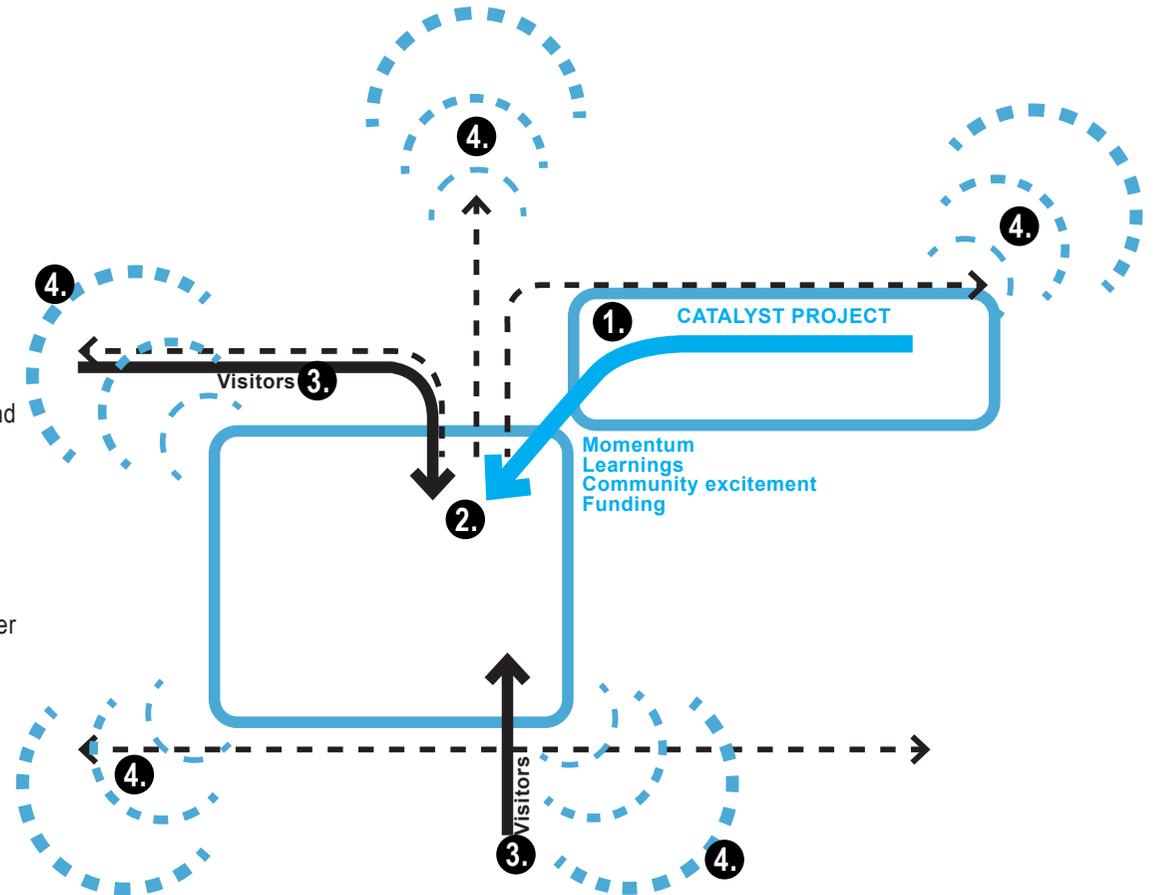
The Pilot Project of the Barrack Street Piazza provides an excellent opportunity to continue the momentum from that project into a more permanent town square space. The Piazza will provide valuable guidance on how the community want to use public space in the town centre.

Development of a town square will meet key desires of the Shire and community including:

- Attract and hold visitors with repeat trips;
- Create a meeting place for locals;
- Showcase that which is unique to Merredin town and Shire; and,
- Will have flow on effects to the broader town centre.

1. Piazza Pilot Project
2. Town Square design and implementation
3. Visits to Town Square
4. Flow on effects to broader town

Figure 7. Catalyst Project Process



- Key Attractors
- Town Centre



Figure 6. Consolidation to town centre attractions

7.6 USES AND PROGRAMMING

Aim: Create a space adaptable for a range of types and sizes of events, for use day, night and all times of the year.

A new town square space can have multiple functions and adapt to a range of uses including:

- Day to day use as a park for picnics, use of playground and public toilets;
- Obvious location for tourists and passers-by to stop for local information, toilets and break from driving;
- Small events within the park;
- Medium to large events within the park and potential for closure of intersection (theatre or music concerts); and,
- Large events with full street closures (annual gala, ANZAC parade etc.)



Figure 8. Small events



Figure 9. Medium events



Figure 10. Large events

7.7 TOWN SQUARE CONCEPT PLAN

The 2009/2017 concept plan is still relevant to guide the Shire in implementing town centre improvements. It is proposed to take advantage of the RAC Pilot Project funding received for the street piazza on Barrack Street, and to use this as a catalyst to develop an activated town centre through the creation of a 'town square'. The plan opposite provides a further level of detail to the town square area at the Barrack Street/Bates Street, space surrounding visitor centre and public toilet, and the connections to this space to the great eastern highway. Feedback from the pilot project should be incorporated into the design.

The Town Square aims to:

- Create a space for pedestrians;
- A green space with shade, turf and garden;
- A meeting place;
- A 'first port of call' for visitors with a staged approach to upgrading the visitors centre and public toilets, and easy access from the Barrack Street car parking and the Great Eastern Highway. Potential for eye catching wayfinding that begin from the town

outskirts and could connect to the silo trail;

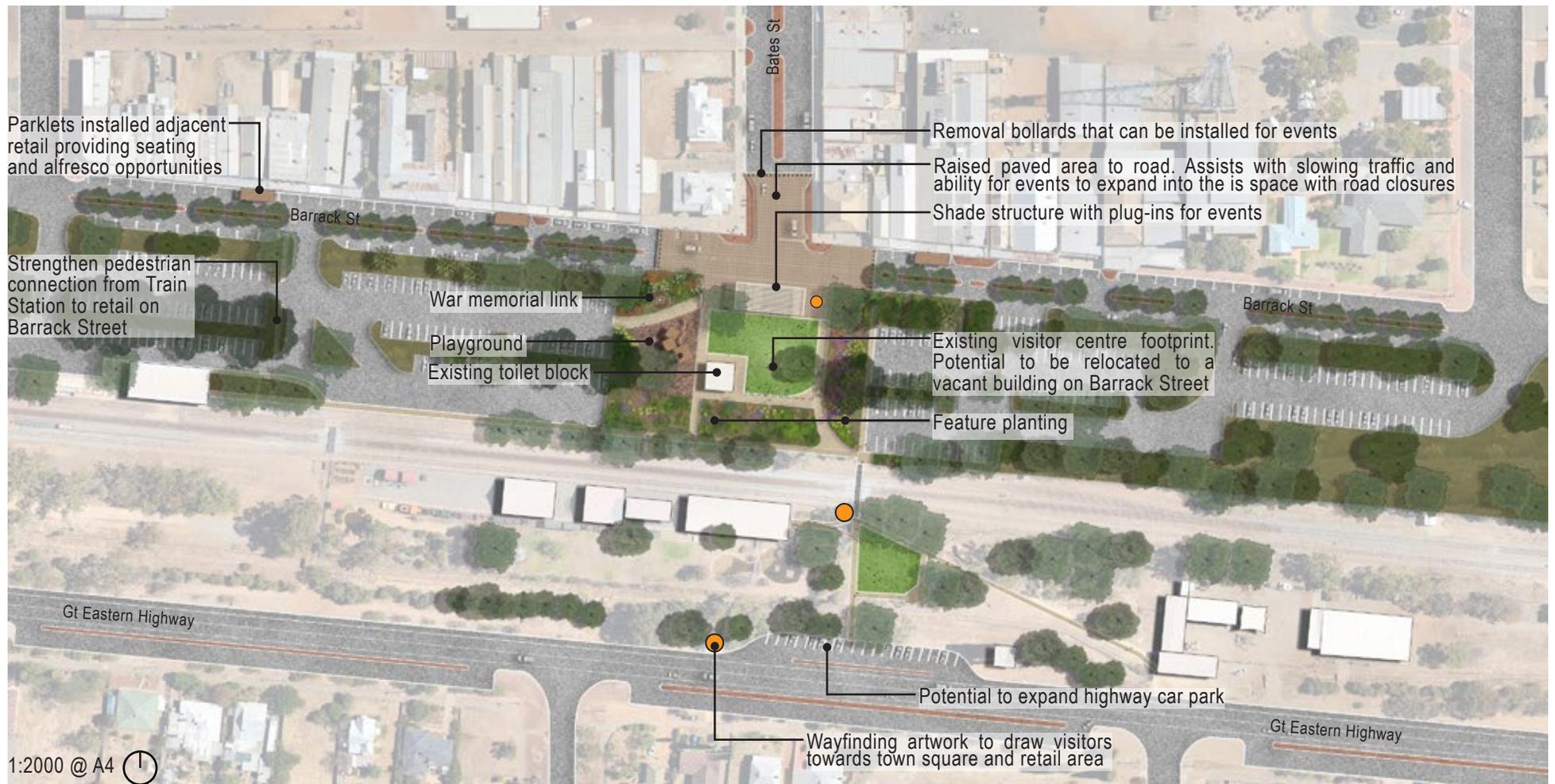
- A flexible events space for small events, markets, or large events such as the annual Gala that may include closing sections of Barrack Street and/or Bates Street;
- A new playground, relocated from APEX park;
- Opportunities to showcase endemic plants including wildflowers;
- Opportunities for showcasing innovation including solar, wind and water. Incorporated into the 'stage', or conceptually through the playground and landscaping.



Figure 11. Town Centre location plan



Figure 12. Proposed Entry Sequence



7.8 EVENTS

A new shade structure is proposed to be located in the town square. The structure should fill a variety of roles, including:

- Providing shelter from the elements;
- Opportunity for integration of art;
- Able to function as a stage or performance space with power and lighting;
- Frame the town square and Bates Street, and be high enough to not block views.

The images adjacent show potential forms and materials from both regional WA and Perth.



Image 8. Forrest Place Perth Source: juliepodstolski.wordpress.com



Image 9. Shade Shelter roof detail, Broome Source:UDLA



Image 10. Roebuck Bay lookout, Broome Source:UDLA



Image 11. South Hedland Source: UDLA



Image 12. Scarborough Beach Source: UDLA

7.9 PLAYGROUND APPROACH

There are a number of factors that will influence the approach to a new playground for Merredin including:

- The Apex Park playground equipment is reaching end of life;
- The desire, by Council and community, for play opportunities for older children, currently not provided in Merredin apart from the skate park;
- The opportunity for play to be included in the development of the town centre, providing an attraction for visitors and the economic benefits that accompany the additional foot traffic for the town centre retail.

The previous community engagement shows a strong preference for nature play and adventure play elements. A new playground provides an excellent opportunity for the creation that is something unique to Merredin and attracts passers-by to stop and spend time in the town.

Scenario 1: Separate areas within Town Centre



Scenario 2: Combined all ages



Scenario 3: Separate areas in Town Square



- New playground in town centre aimed at toddler to 12 year olds.
- Replace Apex Park play equipment with adventure play aimed at 10 to 15 year olds.
- Potentially result in budget being stretched between two sites.
- More difficult for carers to observe different aged children, or for older siblings to supervise younger siblings.

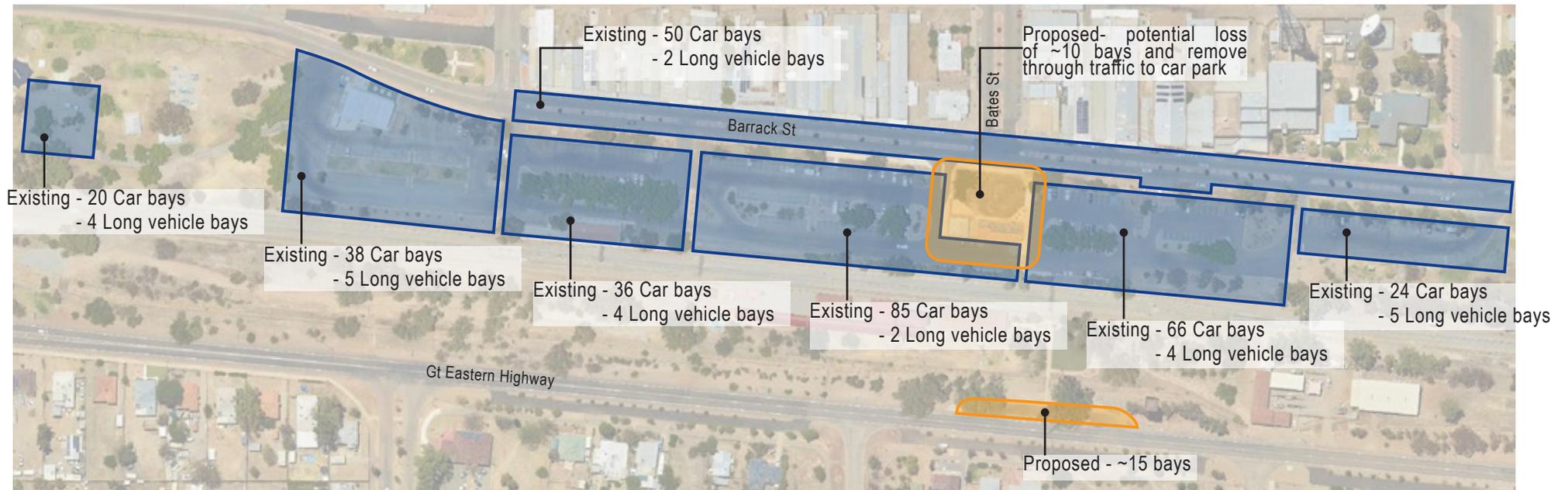
- One large playground for all ages.
- Able to concentrate efforts and budget to one area.
- Opportunity to be a major attraction for visitors, a unique all ages play experience.
- Two playspaces catering for different ages, located to either side of the town square
- Still provides separation for age groups but carers still able to supervise various ages

7.10 PARKING ASSESSMENT

- Existing Parking provision is generous.
- The introduction of the 'town square' would result in minimal loss of bays.
- Potentially a larger parking area could be provided on the highway for visitors who miss the turn off into town to park and walk across the railway line, however a wayfinding scheme should be implemented to encourage use of the main car park.
- Opportunity for implementation of 'parklets' along Barrack associated with alfresco use for businesses. Each parklet fills one on street car bay.



Image 13. Parklet, Wray Ave Fremantle Source:Freosview.com



7.11 REVISED STAGING

The revised staging is based on creating the 'town square' and the connections to this space as a priority.

- Stage 1 - Town Square and Connections
- Stage 2 - Streetscape upgrades
- Stage 3 - Car park arrangement and planting scheme
- Stage 4 - Linear park
- Stage 5 - Great Eastern Hwy planting

